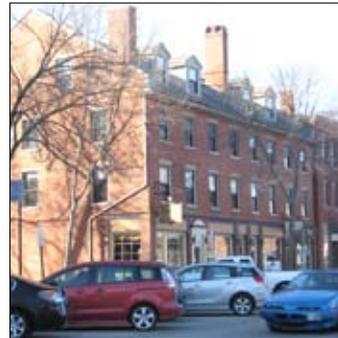




DOVER WATERFRONT DESIGN GUIDELINES

Adopted: November 10, 2008



Prepared for:

Cochecho Waterfront Development
Advisory Committee
Dover Housing Authority
City of Dover, New Hampshire

Prepared by:

The Cecil Group, Inc.

November 10, 2008

CONTENTS

I. INTRODUCTION and SCOPE OF THE <i>DESIGN GUIDELINES</i>.....	1
A. PURPOSE OF THE DESIGN GUIDELINES	
B. NAVIGATING THIS GUIDEBOOK	
II. DESIGN REVIEW PROCESS.....	4
COMPLIANCE	
BACKGROUND	
PROCESS	
COMPLIANCE ALTERNATIVES	
III. GENERAL DESIGN PRINCIPLES AND STANDARDS	6
A. SHARED DESIGN PRINCIPLES	
1. Urban Character	
2. Relationship with the Downtown	
3. Grading and Conformance with Topography	
4. Street and Walkway Connections and Continuity	
5. Varied Landscape Character: Combination of Formal and Informal	
B. SHARED DESIGN STANDARDS	
1. Riverfront Access and Circulation	
1.1 Street in Front of Private Development	
1.2 Street Behind Private Development	
2. Landscaping	
2.1 Street Trees	
2.2 Plant Materials	
3. Lighting	
4. Signage	
IV. PRIVATE SECTOR DEVELOPMENT DESIGN STANDARDS	14
A. ARCHITECTURE	
1. Character	
1.1 Overall Character	
1.2 Styles	
1.3 Common Characteristics	
2. Siting of Buildings	
2.1 Façade Alignment	
2.2 Setbacks from River and Heights	
2.3 Relationship to Riverfront Vehicular and Pedestrian Circulation	
2.4 Setbacks from Streets	

- 3. Massing and Articulation
 - 3.1 Scale
 - 3.2 Massing
- 4. Roofs
- 5. Façade Elements
- 6. Fenestration
- 7. Awnings and Canopies
- 8. Materials
- 9. Colors

B. LANDSCAPING AND OPEN SPACE REQUIREMENTS

- 1. Common Open Spaces
- 2. Pedestrian Access/ Pedestrian Ways
- 3. Site Furniture
- 4. Buffers
- 5. Parking Areas

C. EXTERIOR AND SITE LIGHTING

D. STREETS, DRIVES AND LOADING AREAS

V. RIVERFRONT PUBLIC OPEN SPACE DESIGN STANDARDS23

A. RIVERFRONT PUBLIC OPEN SPACE CHARACTER

B. RIVERFRONT PUBLIC OPEN SPACE DESIGN ELEMENTS

- 1. Open Spaces
- 2. Land Forms and Structures
- 3. Pedestrian Site Circulation
- 4. Water Access
- 5. Gateways
- 6. Paving Materials
- 7. Riverside Boardwalk or Sidewalk
- 8. Pedestrian Character
- 9. Lighting
- 10. Plantings
- 11. Site Furniture
- 12. Signage
- 13. Parking
- 14. Public Art
- 15. Public Restrooms

C. RIVERFRONT PUBLIC OPEN SPACE OPERATIONAL AND MAINTENANCE

- 1. Flood Conditions and Durability

VI. DESIGN STANDARDS FOR PUBLIC WAYS	28
A. STREETS	
1. Dimensions	
2. Crosswalks	
3. Curbing	
B. SIDEWALKS	
C. STREETScape AND LANDSCAPING	
D. ON-STREET PARKING	
E. SIGNAGE	
1. Public Amenity Identification and Wayfinding Signs	
2. Street Signs	
3. Parking Identification Signs	

FIGURES

Figure 1: Plan Diagram: Riverfront Building, Street and Open Space Relationships	9
Figure 2: Section Diagram: Riverfront Building, Street and Open Space Relationships....	10
Figure 3: Section Diagram, Alternative Configuration: Riverfront Building, Street and Open Space Relationships.....	10
Figure 4: Section Diagram: Pedestrian Path and Building Relationships.....	12
Figure 5: Lighting Fixtures.....	13
Figure 6: Open Space Segments.....	24

APPENDIX A: GLOSSARY OF TERMS	A1
--	-----------

APPENDIX B: RELATIONSHIP TO EXISTING REGULATIONS AND DOCUMENTS	B1
---	-----------

1. City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter
2. City of Dover Zoning Code
3. Dover Waterfront Design Task Group Survey Results
4. Cochecho Waterfront Design Charrette 2005 Version

APPENDIX C: RELATED PUBLIC POLICIES, CRITERIA AND REGULATIONS ..	C1
---	-----------

1. City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter
2. City of Dover Zoning Code

- 3. Dover Waterfront Design Task Group Survey Results
 - 3.1 Private Sector Redevelopment
 - 3.2 Public Open Space
 - 3.3 Public Rights-of-way
- 4. Cochecho Waterfront Design Charrette 2005 Version
 - 4.1 Private Sector Redevelopment
 - 4.2 Public Open Space
 - 4.3 Public Rights-of-way

APPENDIX D: ARCHITECTURAL PRECEDENTSD1

I. INTRODUCTION AND SCOPE OF THE *DESIGN GUIDELINES*

A. PURPOSE OF THE *DESIGN GUIDELINES*

These *Design Guidelines* have been assembled to establish standards for the visual qualities of the design and subsequent development of the components of the projects that will compose the Dover Waterfront along the Cochecho River. The *Design Guidelines* are specifically intended to address those design characteristics of the redevelopment directly associated with established public purposes and interests. The design characteristics include:

- Architecture
- Landscape
- Streets and sidewalks
- Infrastructure (visible elements)
- Open Space
- Signage
- Lighting

These *Design Guidelines* have been created to specifically apply to the parcel of land designated as Map 22, Lot 1 on the Tax Map of the City of Dover. This land encompasses three component areas:

- Private Sector Redevelopment – The guidelines will apply to all of land that will be transferred from public ownership to private sector ownership for redevelopment within the Land Disposition Agreement (LDA) between the City of Dover and the designated developer for the site.
- Public Rights-of-Way – The guidelines will apply to all of the land that will be retained by the City for public streets, sidewalks and other right-of-way needs.
- Public Open Space – The guidelines will apply to all of the land that will be retained by the City and improved as public park and open space.

The *Design Guidelines* are intended to establish objective or qualitative conditions that reasonably can be met in the design of improvements and verified through the design review process. They provide a clear framework for design and approval, while deliberately ensuring flexibility in the specific design approach that may be employed in meeting these standards.

There are important distinctions among the *Design Guidelines* for the land components associated with the ownership and jurisdictional roles:

- Guidelines for Private Sector Redevelopment – These standards will be applied through a review and approval process and will be subsequently implemented by the private sector owners and tenants. In addition to fulfilling established public interests, the guidelines are intended to help ensure appropriate design coordination with adjacent improvements in the public rights-of-way and public open space.
- Guidelines for Public Rights-of-Way and Public Open Space – These standards will be applied to the streets, sidewalks, landscaping and other improvements in

public ways. These standards are intended to govern the design and construction of any improvements that may be the responsibility of the designated developer as established in the LDA. These guidelines will also serve as recommended standards for any improvements that may be accomplished by the City. They are also intended to ensure that there is appropriate design coordination with the privately-developed land and the public open space.

The means through which these *Design Guidelines* will be applied in the review, approval and implementation of improvements is described in Section II, Design Review Process.

The *Design Guidelines* do not extend to regulation of projects or improvements under the following circumstances:

- Any design characteristics that are not directly associated with public purposes or interests as formally established by the City of Dover and which are referenced within this document.
- The characteristics of the development that are principally associated with the operational, environmental or construction practices.
- The amount or types of uses that may occur within the Cochecho Waterfront Development, including accessory requirements such as parking.

The *Design Guidelines* have been prepared through the oversight of the Cochecho Waterfront Development Advisory Committee (CWDAC) that is assisting the City and the Dover Housing Authority. These entities have the certain associated responsibilities for oversight of the land transaction and development agreements. The preparation of the *Design Guidelines* has been conducted through a public process including meetings and distribution of drafts of the Guidelines to afford public input. The CWDAC was been provided with professional assistance in preparing these *Design Guidelines* by The Cecil Group, Inc.

B. NAVIGATING THIS GUIDEBOOK

This document will support the design, reviews and approvals of the three components of the Cochecho Waterfront Development. The *Design Guidelines* have been prepared and are presented in a manner that will facilitate the coordination among the components, as well. The following table indicates how different sections of the guidelines may be used as the basis of review of designs and associated approvals, depending upon the project and responsibilities.

Section of the <i>Design Guidelines</i>	CWDAC (Development Coordinator)	Park Design Decision Makers (City)	Public-Right-of-Way Decision Makers (City)
II. DESIGN REVIEW PROCESS <i>Describes the design review and approval process for all Cochecho Waterfront Development components</i>			
III. GENERAL DESIGN PRINCIPLES AND STANDARDS <i>Applicable to all components, to be used to ensure coordinated design</i>			
IV. PRIVATE SECTOR DEVELOPMENT DESIGN STANDARDS <i>Applicable specifically to the private development components</i>			
V. WATERFRONT PARK DESIGN STANDARDS <i>Applicable specifically to the public park components that will be owned by the City</i>			
VI. DESIGN STANDARDS FOR PUBLIC WAYS <i>Applicable specifically to those streets, sidewalks and other improvements in the public rights-of-way that will be owned by the City</i>			
APPENDICES <i>Shared vocabulary of key terms, sources, standards applied through reference</i>			

II. DESIGN REVIEW PROCESS

COMPLIANCE

These *Design Guidelines* provide a range of standards and concepts that are intended to fulfill the public purposes that are under the stewardship of the Cochecho Waterfront Development Advisory Committee and the Dover Housing Authority, through their responsibilities associated with the disposition and redevelopment of the City-owned land that will be within the overall waterfront project. Compliance with these *Design Guidelines* represents compliance with the underlying public interests, except to the degree that overriding regulations, approvals or reviews are deemed to apply.

BACKGROUND

Paragraph 4 (d) of the Term Sheet provides that “all Private and Public Improvements by Developer shall be subject to design approval by DHA and Site Plan approval by the City of Dover Planning Board and such other federal and/or state regulatory agencies having jurisdiction over matters required to be permitted pursuant to the Site Plan (the Public Approvals).

The Land Disposition Agreement (LDA) will include the same provisions.

DHA responsibility is to apply the design standards and the requirements of the Term Sheet and the LDA to review and approve the design of the site. Exercise of this responsibility will be through the activity of CWDAC, which will lead to recommendations to the DHA Board, which, in turn, will have the authority to approve, revise, or reject the CWDAC recommendations.

The Dover Planning Board will exercise its site review responsibilities as provided in Chapter 149 of the City Ordinances. That review involves application of zoning requirements, subdivision and site plan regulations, technical and engineering requirements, and requiring that the Developer comply with state and federal permitting obligations.

While both DHA and the Planning Board have approval authority, DHA is primarily concerned with application of the LDA and Design Standards, while the Planning Board is primarily concerned with application of engineering and regulatory standards.

PROCESS

The design review process is expected to be as follows:

1. The Developer will submit to CWDAC the proposed design for the project. This proposal should include the depiction of: public and private areas; streets, sidewalks, and parking areas; building sizes and locations; building uses, with minimum and maximum uses; phasing; utility design and layout; building design information including elevations and materials, and other information required by CWDAC in order for it to make a recommendation as to whether the proposal complies with the design standards and the LDA. In order for the process to continue, CWDAC would have to vote preliminary approval of the design.

2. If CWDAC gives preliminary approval of the design, then the Technical Review Committee would review the site review application to review whether the application complies with the technical requirements of engineering and regulatory matters. The TRC will conduct its review and provides comments and recommendations for design revisions.
3. CWDAC would then review the recommendations from TRC to determine if CWDAC would still recommend approval if the design would have to be modified to reflect the comments and recommendations from TRC. Based on its further review, CWDAC would then vote on a final recommendation as to whether the revised design complies with the design standards and the LDA.
4. The CWDAC recommendation would then be reviewed by the DHA Board at its next regularly scheduled meeting and the DHA Board would then vote to either approve or reject the CWDAC recommendation.
5. The Planning Board would conduct its own full review of the subdivision and/or site plan application, to apply the regulations and requirements which it customarily applies. This review may result in a revision of the proposed development.
6. If the Planning Board votes to approve the subdivision and/or site plan application, then within fifteen days of the Planning Board's vote, CWDAC and DHA would again review that approved plan and the conditions of approval to determine if the revised plan complies with the design standards and the LDA.

Final approval would include any state or federal approvals. If conditions imposed on those approvals required revision of the design, the Planning Board may choose to review the revisions. CWDAC/DHA would conduct further proceedings to determine if the respective agency would approve the design with the new requirements imposed by either the state or federal agencies. Any final CWDAC/DHA approval shall be given or denied within fifteen days of request.

COMPLIANCE ALTERNATIVES

While these *Design Guidelines* provide useful standards for approval of this complex, phased project, they are not intended to preclude design solutions that both the City (through the CWDAC) and the proponent for the private sector development mutually determine are appropriate. Rather than requiring amendments to the *Design Guidelines*, compliance alternatives for any of the specific guidelines in this document will be considered fully acceptable if they are defined in written form and approved by both parties as part of any formal review and approval process.

III. GENERAL DESIGN PRINCIPLES AND STANDARDS

The following general design principles are equally applicable to any project within any of the three components of the Dover Waterfront Development (private sector redevelopment, public rights-of-way, and public open space). The first part of this section describes general principles that should inform the character and quality of all elements. The second part of this section provides more specific *Design Guidelines* that are shared among the three components.

A. SHARED DESIGN PRINCIPLES

1. Urban Character

The composition of the elements within the Dover Waterfront Development should attain an urban character by accomplishing the following principles:

- Building to street orientation - Buildings should have a principal entrance along the principal street (Washington Street), interior streets or major interior vehicular circulation ways that attain the appearance and character of streets.
- Limited views of surface parking - Open surface parking should be obscured behind or below buildings, integrated into the streetscape along the edges of streets as either parallel, angled or perpendicular spaces or should be substantially screened from public views as provided for in specific design standards in these *Design Guidelines*.
- Formal and continuous streetscaping for each street - The landscape and streetscape elements should have the same uninterrupted and regular patterns of paving, street trees, and streetlighting along each of the principal street (Washington Street), all other public streets and interior streets intended for public use. This is not intended to imply that the streetscaping for all streets must be the same; rather, that each street must have an identifiable, formal and continuous streetscape character.
- Sidewalk and walkway paving materials and patterns - The sidewalk and walkway materials and patterns shall be the same along any contiguous segment of the overall pedestrian circulation network that is intended for public use and access, regardless of whether it is located on public or private property.

2. Relationship with the Downtown

- The Downtown as visual reference source - The design of the major elements should be demonstrably related in style, materials and colors to observable existing improvements in the adjacent downtown, or to historic elements that have been removed but that are adequately documented through historic photographs or other images. Evidence of this relationship will be accomplished by presenting photographs of relevant, related improvements as part of the approval process. Clear reference to the composition and elements of established styles is intended to ensure a sense of architectural integrity with each building, and compatibility with the integrity of the traditional architectural styles that are valued within the Downtown. Examples of buildings and building elements that could serve as visual references for new designs has been included in a photographic inventory provided in Appendix D

to these Guidelines. Designs could refer to these or other appropriate examples provided by the proponent during the design review process.

- Avoiding literal reproductions of historic buildings – The design of new buildings that use traditional architectural styles should not literally mimic or reproduce exact designs from the past, but should be recognizable as buildings with their own integrity and expression.
- Providing for compatibility and flexibility in color – Regarding color, the objective of these guidelines is to provide for a reasonable compatibility between the predominate colors for buildings so that they may be associated with traditional styles and the range of tones and hues within the downtown. However, the colors associated with decorative elements and with shops or restaurants are traditionally and appropriately more varied and lively; these Guidelines should not be interpreted to limit flexibility for such features.
- Flexibility to provide contemporary compositions – The use of the Downtown as a visual reference as described above is not intended to preclude contemporary approaches or interpretations in assembling the components of any portion of the development, or the incorporation of contemporary materials as permitted within these *Design Guidelines*.

3. Grading and Conformance with Topography

The layout of all improvements should be designed to generally follow the existing topography of the site. The layout of roads, walkways and building footprints should be aligned with existing contours where practical, with limited connecting streets or walkways aligned perpendicular to existing slopes.

4. Street and Walkway Connections and Continuity

All streets and pedestrian routes intended for public use should be of the same width and materials at their connections and be of consistent alignments and lighting levels, regardless of the ownership of these connecting streets and paths.

5. Varied Landscape Character: Combination of Formal and Informal

As an extension of the downtown, the streetscapes and portions of the public open space along the riverfront should be urban and formal in character. This should include regularly spaced street trees along Washington Street, River Street, and the River Street extension that will be parallel to the river, until it reaches the “node” where a pier is to be located.

Other than formalized landscaping patterns that may occur along the waterfront park and streets, the landscape character should be varied and associated with individual buildings and spaces, in keeping with the New England character of the Downtown landscape of Dover, rather than shared landscape patterns more typical of campuses or large building complexes.

B. SHARED DESIGN STANDARDS

Many of the design standards for elements within the three components of the Dover Waterfront Development (private sector redevelopment, public rights-of-way, and public open space) will be shared. The following list describes the shared standards.

1. Riverfront Access and Circulation

The riverfront access and circulation is likely to require coordinated application of Design Standards, depending upon the final layouts and alignments. The following standards are provided to encompass the range of acceptable solutions.

1.1 Street in Front of Private Development

Street access between the riverfront public open space and private development is an acceptable site design solution, if the component elements accomplish the following guidelines, some of which are also illustrated in Figures 1, 2, and 3:

- Public open space depth – The minimum depth of the public open space shall be no less than 40 feet as measured from the highest observable tide line to the curb line of the street, except as provided for in Section IV-A-2.2 & 2.3. The highest observable tide line shall be defined in compliance with the State of New Hampshire (NHDES) definition (RSA 483-B:4).
- Sidewalk – A sidewalk at least 6 feet wide must be provided along the riverside of the street, but should be separated by a landscaped strip from the curb except at crosswalks and special pedestrian areas that may extend across the street such as “tables”. A sidewalk at least 8 feet wide must also be provided along the upland side of the street. The sidewalks and paths along these street segments should be aligned with the streets.
- Pedestrian character – The design of the street and sidewalks should employ materials and features that diminish the appearance of a standard street and emphasize a pedestrian character. This may include pedestrian “tables”, special paving materials, and alternatives to curbing such as bollards or posts or other features.
- Roadway width – The maximum roadway width, exclusive of parking spaces, is 22 feet.
- Parking – Parallel or angled parking may be provided on the upland side of the street only; no parking will be permitted on the riverside of the street, except for limited parking spaces that may be provided to the north and west of the “node” which is planned to include a pier and small pavilion.
- Street trees – Street trees must be located in a regular pattern along the riverside and the landside.
- Streetlighting – Street lighting may be located on either side of the street, or in an alternating pattern.

FIGURE 1: Plan Diagram: Riverfront Building, Street and Open Space Relationships

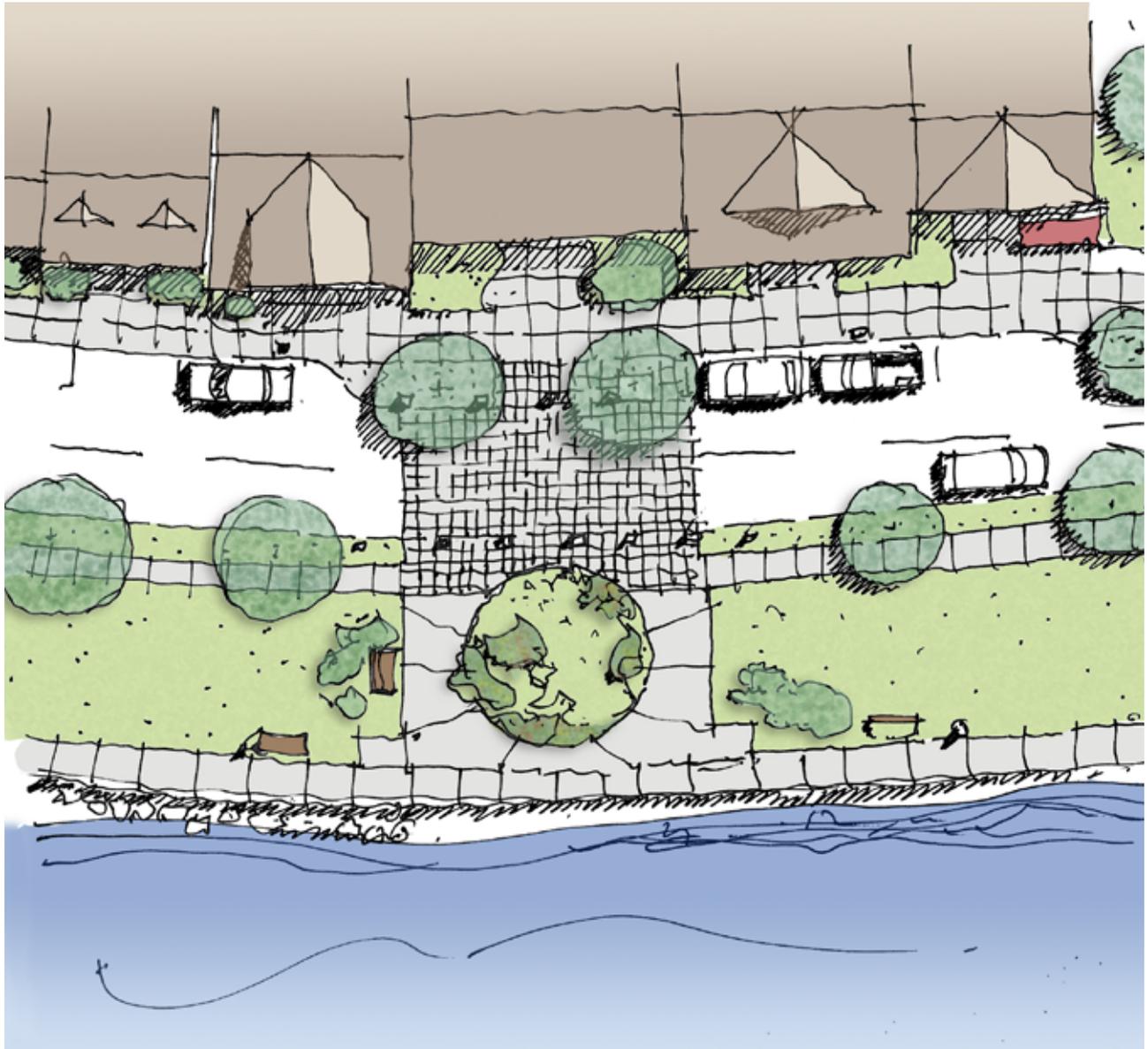


FIGURE 2: Section Diagram: Riverfront Building, Street and Open Space Relationships

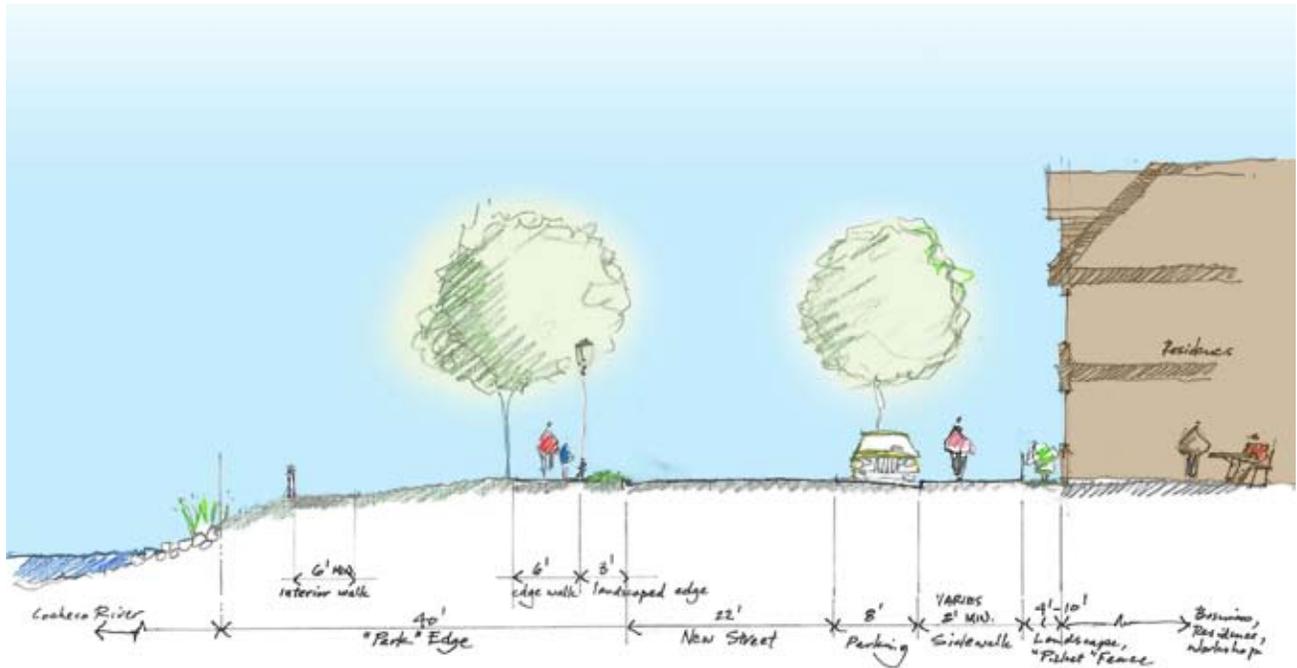
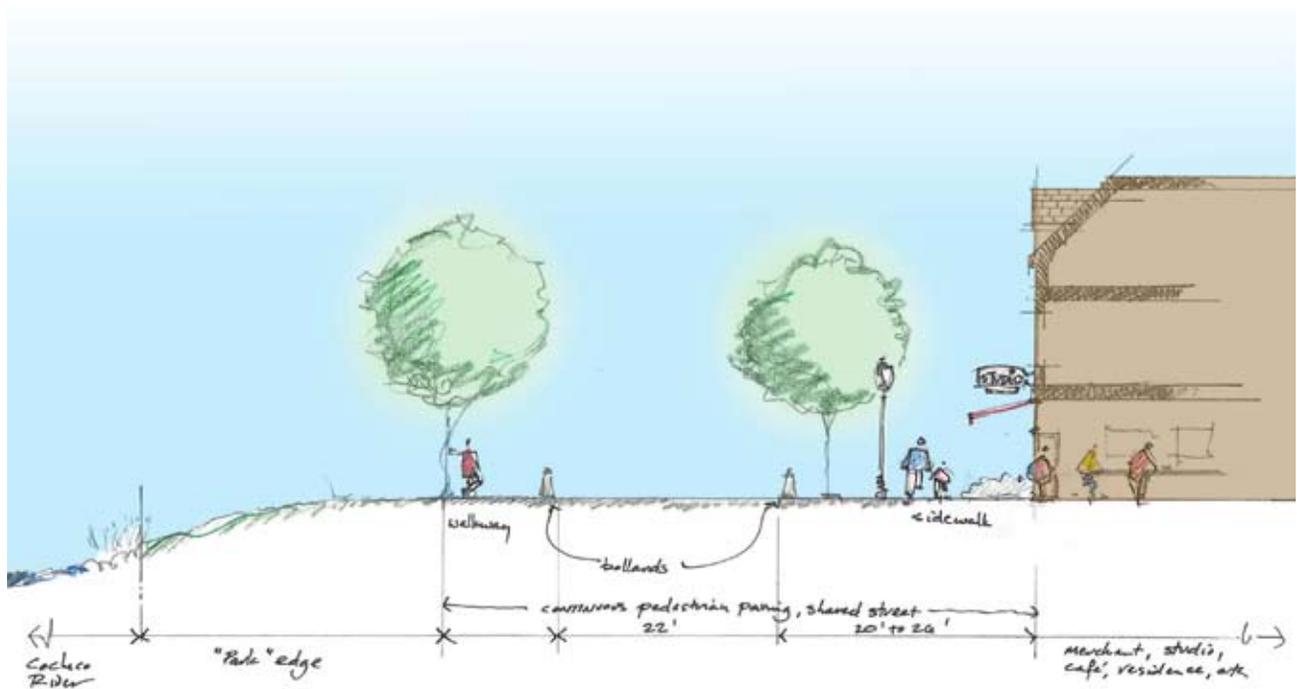


FIGURE 3: Section Diagram, Alternative Configuration: Riverfront Building, Street and Open Space Relationships



1.2 Street Behind Private Development

Street access may be provided behind private development that is located along the riverfront, if the following standards are met. In addition, Figure 4 indicates related minimum standards.

- Commercial space access - Private development of commercial uses must provide at-grade access from the riverfront side for patrons.
- Residential access – Primary access to residential uses must be provided from the street (upland) side. However, access to private residences may not occur along the riverfront side; at least a 3 foot grade separation must be maintained from public riverfront walkways and the ground floor of residences.
- Riverfront promenade – A continuous riverfront promenade must be established along the riverfront that is at least 20 feet from private residential buildings and separated by a landscaped area, and include lighting for pedestrian safety and nighttime use.
- Roadway width – The maximum roadway width, exclusive of parking spaces, is 22 feet.
- Parking – Parallel, perpendicular or angled parking may be provided on either side of the street; no parking lots will be permitted on the riverside of the street. The design of parking garages and parking courts for riverside units should include landscaping, fencing or other measures to diminish the visual impact of these parking areas.
- Sidewalks – Sidewalks should be located on only one side of such a street segment,
- Street trees – Street trees must be located in a regular pattern along both sides of the street.
- Streetlighting – Street lighting may be located on either side of the street.

2. Landscaping

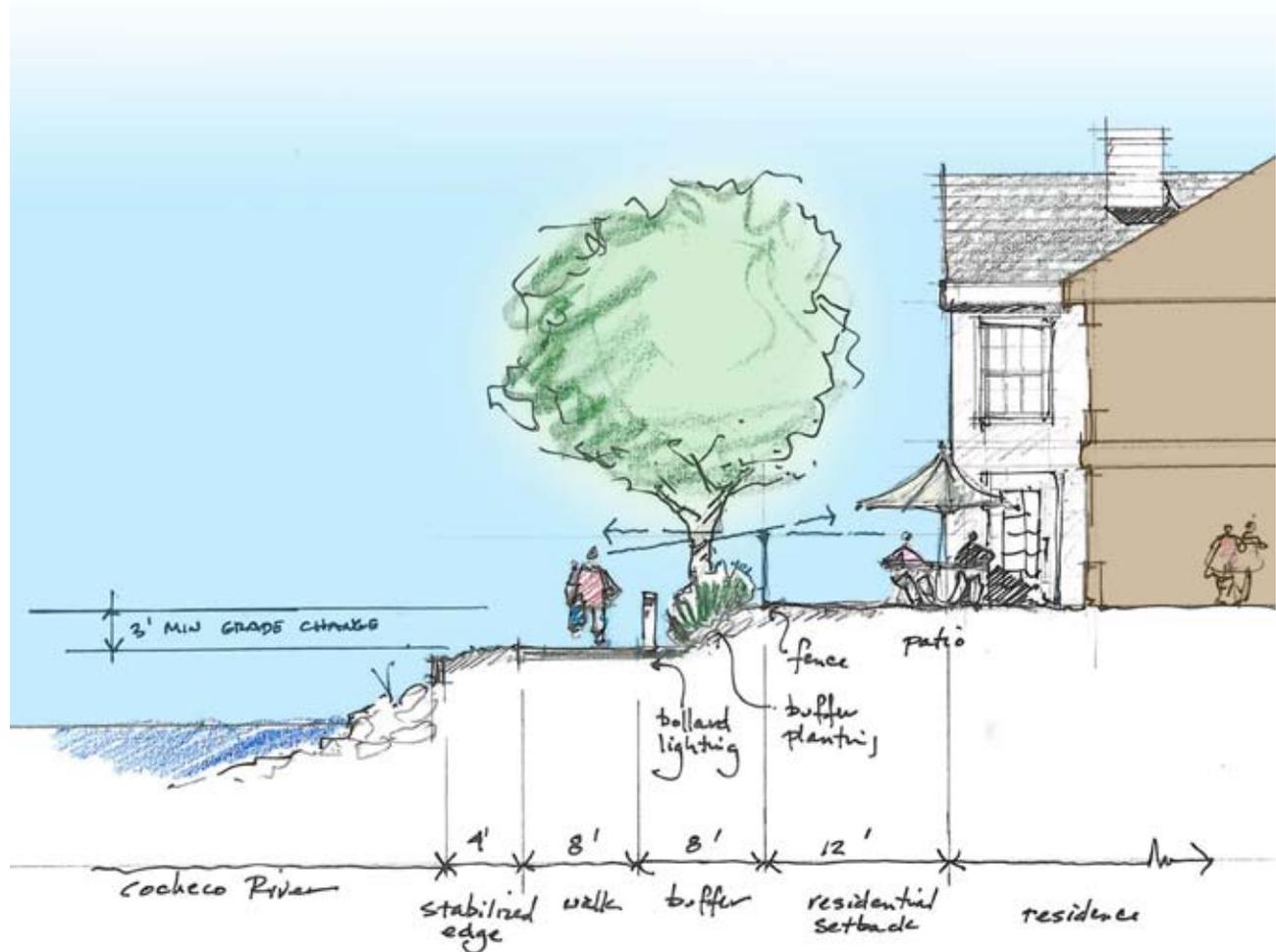
2.1 Street Trees

Street tree species, to the extent that they are provided along any street shall be continuous and of the same species, regardless of whether they are located on public or private property. The tree species along Washington Street shall be consistent and be chosen from trees that are found to the east of the site. Tree species along River Street and the River Street extension shall be chosen from among the trees along the River in Henry Law Park, or chosen from either *Ulmus americana* (American elm), or *Quercus rubra* (red oak).

2.2 Plant Materials

No prohibited invasive species shall be permitted. Refer to the list of invasive species maintained by the New Hampshire Department of Agriculture. While specific plants are not required, the following are guidelines:

FIGURE 4: Section Diagram: Pedestrian Path and Building Relationships



Maintained ground covers and lawns shall be regularly maintained and accompanied by a maintenance program.

- Seasonal plantings – Seasonal plantings of any species may be undertaken if accompanied by a maintenance program.
- Residential units - Plantings on decks, balconies, window boxes or private gardens associated with residential units may be of any species.

3. Lighting

The streetlighting fixtures shall be the same for all streets within the Cochecho Waterfront Development regardless of ownership of the land on which they are placed. Street lighting fixtures shall be LUMEC Contemporary Lantern 81 Series or similar. Where pedestrian level lighting is employed as bollard lighting, LUMEC CANDB1 or similar fixtures shall be used. These fixtures are shown in Figure 5.

The illumination levels and mounting heights shall comply with the following standards:

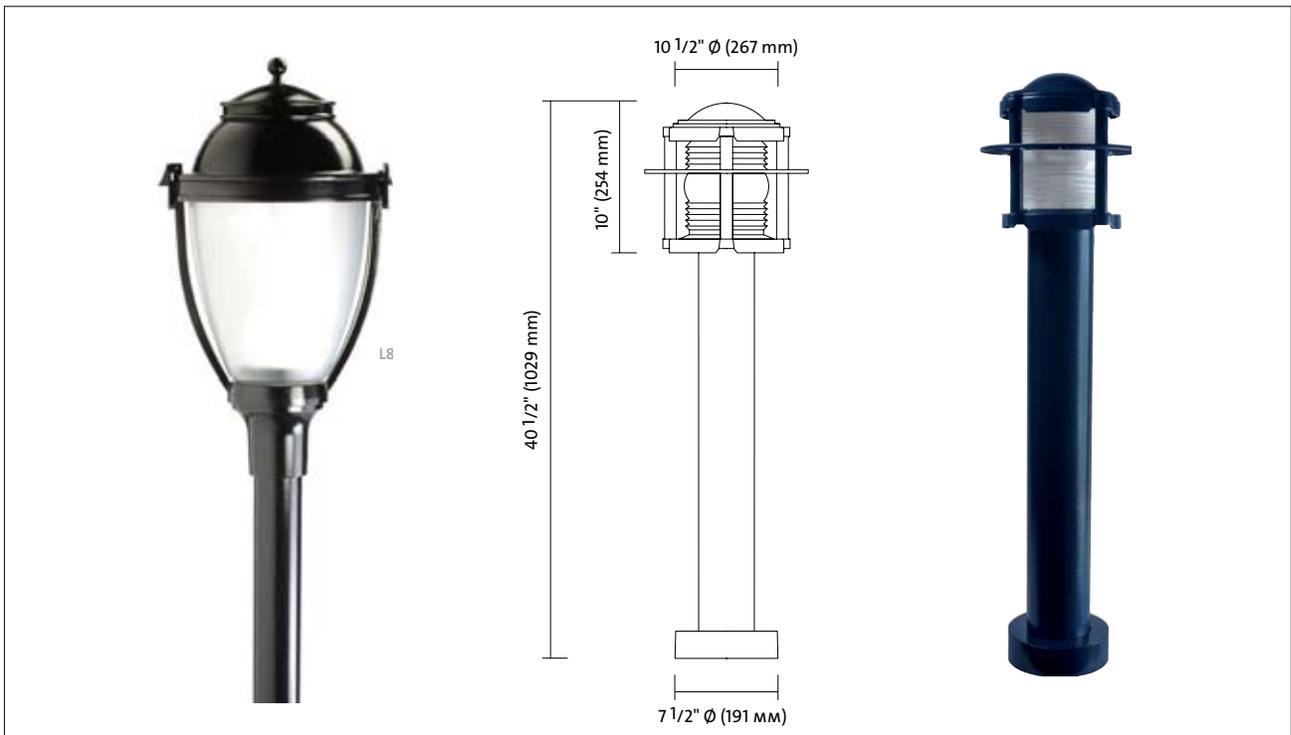
	Light Source	Mounting Height (approx.)	Minimum Maintained Average Values (fc)	Uniformity Ratio (Eavg/Emin)
Washington Street	Match City standards	16	0.9	4.0
River Street and River Street Extension	Metal halide	12	0.9	6.0
Other interior streets	Metal halide	12	0.9	6.0
Primary pedestrian paths along streets	Metal halide		0.6	4.0
Waterside sidewalks and/park paths	Metal halide		0.2	4.0

A minimum average maintained lighting level of 0.6 footcandles should be provided along the public sidewalk on the landside of such a street, with a uniformity ratio of 4.0 as defined by the International Engineering Society of North America (IESNA).

4. Signage

The provisions of the City's Zoning Ordinance are appropriate to guide the design of signage within the district.

FIGURE 5: Lighting Fixtures



IV. PRIVATE SECTOR DEVELOPMENT DESIGN STANDARDS

A. ARCHITECTURE

1. Character

1.1 Overall Character

The architecture of the buildings within the entire Cochecho Waterfront Development (the “Development”) should be varied among the different types of uses and include variation among the architectural styles. This will be accomplished through the following standards:

- Limited repetition for townhouse structures – Repetitive architectural design with the identical façades and identical materials is limited to townhouses (attached single family residential). However, identical building designs should not become so repetitive among adjacent townhouses or among identical clusters of townhouses so that the goal of variety and diversified scale are compromised.
- Prohibition on identical or mirrored architecture for large buildings – Buildings other than townhouses as noted above are prohibited from having identical or mirrored architectural expression with another building at the Development. There must be adequate differences in the massing, articulation, roof forms, color, materials, style or other aspects of the buildings to readily distinguish the different appearance.
- Incorporation of downtown characteristics – Each building must demonstrate that it incorporates characteristics that are visually similar and are associated with either an existing downtown building or historic building that has been demolished. This must be demonstrated by the use of photographs or other images, indicated shared characteristics in terms of roof forms, massing, building style, primary exterior material, fenestration style, or colors.
- Sensitive to the environment – Building designs are encouraged to incorporate environmentally sensitive and energy efficient technology and materials

1.2 Styles

The architecture of the buildings must be derived from architectural styles that exist today or which were historically present in Dover’s downtown.

- Relationship to downtown architecture – The proponent must demonstrate that each building meets this requirement by providing an image of one or more precedent buildings, identifying its style, and by indicating that the proposed building is derived from that style by virtue of any of the following applicable qualities: Massing, composition, fenestration patterns, ornament and details, materials, roof lines and roof forms. The City’s representatives must find that the proposed building precedent(s) represent the character and quality of design that is consistent with the purposes of these *Design Guidelines*. Alternatively and if both parties agree, precedent(s) from other communities may be used to as source of architectural character. Appendix D contains an extensive group of photographs to indicate a few of the architectural styles and compositional elements among the many excellent precedents available as sources for design inspiration.

- Contemporary interpretation – Building architecture should not be a reproduction of existing buildings or all aspects of any traditional style; they should have contemporary elements or interpretations associated with contemporary materials, building methods, or use requirements.

1.3 Common Characteristics

- Ground level articulation along riverfront and major interior street - The facades of buildings along the riverfront or the major interior street should have a clear articulation of the ground level, by virtue of distinctive colors, materials, ornament or details such as banding to accentuate the pedestrian orientation of these areas of the site.

2. Siting of Buildings

The following standards apply to the siting of buildings:

2.1 Façade Alignment

The following standards apply to the alignment of buildings and their facades.

- Orientation of facades with public or main entrances - Buildings should be aligned so that the facades that contain the principal public or main entrances are approximately aligned with the street that they face. Secondary entrances may be provided at any other location and orientation.
- Staggered setbacks of townhouses – The staggering of townhouse units should be limited to locations where it provides appropriate relief or improves the orientation of views from the units. To implement this principle, at least 40 percent of the facades of townhouse units should be aligned with an adjacent unit.

2.2 Setbacks from River and Heights

Siting of buildings should respect setbacks from the river in terms of the number of floors. The setback from the river is measured from the highest observable tide line as defined in compliance with the State of New Hampshire (NHDES) definition (RSA 483-B:4).

Condition	Setback from Cochecho River (minimum)	Height in Floors
Water dependent Use	15 feet	1 floor
Building (any use)	35 feet	2 1/2 floors
Building (any use)	50 feet+	3 floors
Building (any use)	100 feet	4 floors

+ Except as provided for in Section 2.3

Notes: Building heights as recommended in the Cochecho Waterfront Design Charrette 2005 Version should not be employed to assess compliance with this standard; this would have the effect of discouraging sloped roofs and other massing alternatives that are specifically sought in other standards.

Parking levels, when provided partially below grade and as defined in Appendix A - Glossary, are not considered as floors for the purposes of these calculations.

2.3 Relationship to Riverfront Vehicular and Pedestrian Circulation

Buildings along the Riverfront may have several orientations, depending upon the resolution of the site plan and definition of the relationship between the public park space and the private development area. The following relationships represent acceptable outcomes.

- Commercial use: Washington Street corner and public open space relationship – The southwest corner of the Development should include a retail or restaurant use as defined within the Land Disposition Agreement (LDA). This building should have facades aligned along the riverfront, Washington Street and other streets that it may have frontage along; the building may not be separated from the riverfront by a street. Setbacks from Washington Street should be no greater than 25 feet, and the setback from the riverfront should be no greater than 40 feet.
- Residential or mixed-use building upland of an extension of River Street – In the event that buildings are sited upland of a waterfront Street with no intervening buildings between them and the park, then the buildings must be set back no less than 4 feet and no more than 10 feet from the sidewalk. If the uses are residential on the ground level, floor levels may be at grade or separated vertically. If the buildings are intended for retail, commercial or flexible use, then the entrances must be at grade.
- Building along the waterfront park - In the event that there are buildings along the waterfront park without an intervening Street, then they must comply with the setback requirements listed in Section III General Design Principles and Standards B.1.2.

2.4 Setbacks from Streets

Except as otherwise provided in these *Design Guidelines*, setbacks from Streets for the majority of any building façade with a principle entrance shall not be greater than 20 feet.

3. Massing and Articulation

3.1 Scale

In order to mitigate the scale of larger buildings or groups of buildings, the following standards shall apply.

- Articulation of individual townhouse units – The facades and roofs of individual townhouse units must be designed to clearly convey that they are separate residences through the use of entrance expressions, fenestration patterns, staggered setbacks, ornamentation, roof forms or other means.
- Façade articulation - Large buildings (greater than 10,000 square feet) must be relieved by the introduction of elements that create articulation of the principal mass. This can be accomplished by changes in materials, colors or by changes in the plane of the façade that represent at least 25 percent of the façade area along any elevation that is longer than 70 feet in length.

3.2 Massing

The following standards apply to the massing of buildings

- Typical Heights - Buildings must be between 2 and 4 floors in height unless specifically agreed to by the CWDAC.
- Single Floor Buildings – Buildings provided for retail or restaurant use along the waterfront may be single floor structures.
- Limits on ground level parking – No parking will be permitted on the ground level of any building, unless it is part of a townhouse or is partially below grade as described below.
- Standard for below-grade parking – Parking levels below occupied floors of buildings are allowed if they are completely or partially concealed below grade. Parking levels will not be considered to be at grade if the finished floor level is at least 4 feet below the grade along one side of the building, and at or below the surrounding finish grade on all other sides.

4. Roofs

Roofs shall have forms that are similar to those associated with the downtown architectural style that serves as any building's precedent. Rooftop mechanical equipment will be screened with architectural elements, if visible from the street level.

5. Façade Elements

Façade elements shall include characteristics associated with the downtown architectural style that serves as any building's precedent.

6. Fenestration

In general, façade elements shall include characteristics associated with the downtown architectural style that serves as any building's precedent.

- Buildings with non-residential ground level uses along the waterfront park – For facades of buildings with non-residential uses on the ground level that directly face the waterfront park, at least 50 percent of the ground level will be composed of transparent glazing as measured within a band between 2 and 8 feet above the adjacent grade.

7. Awnings and Canopies

Awnings and canopies must be fabricated of opaque materials; internally lit translucent awnings or canopies are not permitted.

8. Materials

The materials employed as the predominant exterior materials should be consistent with the architectural style employed as the precedent(s) for any building. The following additional standards shall be applied.

- Allowed primary façade materials - Predominant exterior materials may be composed of wood, brick, stone, cast stone, fiber reinforced concrete or stucco with a stone-like exterior finish, textured concrete block (not including split-faced block), and terra cotta or fabricated materials which, effectively replicate the character of these materials based on submitted and approved samples.
- Prohibited primary façade materials – Prohibited for use as predominant exterior materials are vinyl and aluminum siding arranged in a horizontal clapboard pattern, metal panels, glazing including spandrel glazing.
- Trim, ornament and secondary materials - Trim, ornament and secondary façade materials may be composed of any of the materials permitted as primary façade materials. In addition, metal or coated metal may be employed.

9. Colors

Colors of exterior elements shall generally be those associated with examples of the downtown architectural style that serves as any building's precedent. In addition, the following standards apply.

- Primary façade material colors – The primary façade materials should be visually similar to existing downtown building colors used as any building precedent as represented by color samples and photographs of the existing building or buildings used as its precedent.
- Colors for trim, ornament and secondary materials – Trim, ornament and secondary façade colors may vary from the precedent building color in order to introduce variety or contemporary themes.
- Colors for retail or restaurant facades, canopies, or awning – The colors that may be associated with those portions of a façade devoted to retail or restaurant uses are unrestricted.

B. LANDSCAPING AND OPEN SPACE REQUIREMENTS

1. Common Open Spaces

Common open spaces consist of landscapes and streetscapes within the privately-owned land that are intended to serve as publicly-accessible areas such as plazas and small park-like environments and include pedestrian paths that are directly connected to the waterfront park or sidewalks along Washington Street or the main interior street.

Where such areas shall be created within the Development, they will include at least two benches each and widened paving at least 8 feet clear width to accommodate seating. At least 40 percent of the paving within plazas or park-like environments shall be brick or unit pavers.

2. Pedestrian Access/ Pedestrian Ways

The following specific standards apply to the pedestrian network:

- Sidewalks - Sidewalks will be provided along both sides of each street unless otherwise directed within in these *Design Guidelines*. Sidewalks must be provided adjacent to the curb if there is adjacent on-street parking. In other locations, sidewalks may be either adjacent to the curb or separated by a landscaped strip at least 4 feet wide.
- Public pedestrian ways - Additional pedestrian ways available to the public and provided as part of a continuous pedestrian network may be provided at the discretion of the designated developer. All pedestrian ways intended for public use connected to the public pedestrian network and sidewalks will be at least 6 feet wide and include lighting.
- Paving materials – Pedestrian ways intended for public use and sidewalks may be composed of concrete, brick or concrete unit pavers. In the event that concrete is used, at least 5 percent of the paving must be composed of brick or concrete unit pavers to provide variety and to accentuate special areas.
- Other pedestrian paths – Pedestrian paths intended primarily for residents or associated with loading, maintenance or other purposes may be provided at the discretion of the designated developer and may be constructed of any suitable material, including asphalt.

3. Site Furniture

Acceptable site furniture will consist of manufactured elements of which examples may be found that are the same or similar within Dover’s downtown. In addition, the following shall apply:

- Fencing – Low fencing (no greater than 60” above grade) must be provided to separate any residential use that is adjacent to public park land. Other fencing may be used to protect planting areas, provide for security or screening, or as a landscape element. Fencing used for screening may be no taller than 8 feet. Other fencing may be no taller than 42” above grade. Painted metal, metal or wood fencing may be employed; chain link fencing may be used only in locations where it is required as a barrier for safety purposes, and must be coated with black vinyl or other approved material. Any other materials for fencing shall be subject to approval of the CWDAC.

4. Buffers

The following guidelines are intended to enhance the visual quality of the vehicular-oriented parking and circulation areas.

- Landscaped Buffer Strips - A buffer strip shall be provided separating, parking areas, vehicular circulation facilities, or similar improvements from the sidewalk along any public street, or any private way which is judged to perform an equivalent function. The landscaped buffer strip shall be continuous except for required vehicular access points and pedestrian circulation facilities. Signs shall be designed to be integrated into the landscaping. Landscaped buffer strips shall not be less than ten (10) feet in depth unless the CWDAC allows narrower buffer strips if it

is demonstrated that an adequate visual buffer will be maintained and that there would be no detrimental effect on the long-term viability of the landscaping and plantings within the buffer strip.

- Buffer Strip Composition - The buffer strip shall include a combination of deciduous and/or evergreen trees and lower-level elements such as shrubs, hedges, grass, ground cover, fences, planted berms, brick or stone walls. The buffer strip is intended to provide substantial screening, but is not intended to entirely conceal parking areas from visibility from streets or publicly-accessible sidewalks. At least one (1) tree shall be provided per twenty-five (25) linear feet of street frontage or portion thereof. There shall be a minimum of three (3) trees in the entire buffer strip. Trees may be evenly spaced or grouped. Groups of trees shall be spaced no further apart than fifty (50) feet. At least ten (10) shrubs shall be provided per one hundred (100) square feet of landscaped area in the buffer strip.
- Opaque Screens for Loading and Services Areas - An opaque screen may be comprised of walls, fences, berms, or evergreen plantings, or any combination thereof, and should be provided to conceal loading areas and service areas such as dumpsters where they would otherwise be visible from streets or publicly-accessible sidewalks. Opaque screens shall be opaque in all seasons of the year. The screen shall be opaque from the ground to a height of between two and one-half (2 1/2) to six (6) feet when planted or installed as determined by those responsible for implementing these Guidelines.
- Berms - When berms are used to meet the requirements for a buffer strip they shall be planted with living vegetation. The minimum top width of a berm shall be three (3) feet, and the maximum side slope shall be 3:1. No more than twenty-five per cent (25%) of the coverage of a planted berm shall be mulch or non-living material.
- Mulches - When used in required landscaping or buffers, mulches shall be limited to bark mulch or decorative stone. No more than twenty-five per cent (25%) of the coverage of the landscaped area shall be mulch or non-living material.
- Intersection Sight Distance Restrictions - Landscaped buffers and screening shall not restrict sight distances at intersections or driveway entrances. Sight distance requirements, location and specification of site zones shall be determined by reference to a current edition of an applicable manual of roadway engineering design principles.
- Standards for Plant Materials – Standards for plant materials shall be the same as for Parking Areas, below.
- Design for Pedestrian Circulation – *Design Guidelines* for pedestrian circulation shall be the same as for Parking Areas, below.

5. Parking Areas

The following guidelines are intended to enhance the visual quality of the vehicular-oriented parking and circulation areas, to encourage the creation and protection of open space, to avoid expansive development of impervious surfaces, and to ensure that landscaping is an integral part of development. Landscaping within parking areas shall provide visual and climatic relief from broad expanses of pavement and shall be designed to define logical areas for pedestrian and vehicular circulation and to channel such movement on and off the site. Where practical and feasible, existing healthy, mature vegetation shall be retained in place.

- Applicability - The requirements shall apply to any parking area containing ten (10) or more parking spaces and buffers strips and screening of loading and service areas such as dumpsters as described below.
- Scale - Parking areas shall be broken into sections not to exceed fifty (50) cars per section. Sections shall be separated by landscaped buffers to provide visual relief. At a minimum, the buffers shall consist of islands which shall be a combination of “divider islands” and “terminal islands”.
- Islands - Each landscaped island shall have a minimum area of one hundred fifty (150) square feet and shall consist of pervious landscaping. Curbing, at least five (5) inches in height, shall surround each landscaped island as protection from vehicles. No tree shall be planted less than four (4) feet from the curbing. Grass or ground cover may be substituted for shrubs in divider islands and terminal islands if deemed appropriate by those responsible for the review and approval of the design.
- Divider Islands - At least one landscaped divider island shall be provided for every four (4) parallel rows of parking. Trees shall be spaced not more than twenty-five (25) feet on center. At least one (1) shrub shall be provided for every five (5) linear feet, or one (1) shrub per thirty-five (35) square feet of ground area, whichever results in a greater number of shrubs.
- Terminal Islands - Landscaped terminal islands shall be provided at the ends of rows of parking where such rows are adjacent to driveways or vehicular travel lanes. In addition, terminal islands shall separate groups of parking spaces in a row, such that no continuous line of adjoining spaces contains more than twenty (20) parking spaces. Terminal islands shall be used either (1) to separate parking spaces from driveways and other vehicular travel lanes, or (2) to break up large numbers of parking spaces in a single row of spaces. Terminal islands shall contain at least one (1) tree when abutting a double row of parking spaces.
- Standards for Plant Materials - Deciduous trees must be at least two and one-half (2-1/2) to three (3) inches caliper, six (6) inches above the top of the root ball, at the time of planting; and must be expected to reach a height of at least twenty (20) feet within ten (10) years, when considering the expected normal growth patterns of the species. Evergreen trees must be at least eight (8) feet in height at the time of planting. Ornamental or specimen trees must be at least eight (8) feet in height at the time of planting. Shrubs and hedges must be at least three and one-half (3-1/2) feet in height or have a spread of at least twenty-four (24) inches at the time

of planting. Shade or canopy trees shall be provided within parking lots, and within buffer strips.

- Design for Pedestrian Circulation - Buffers and parking areas shall be designed to include appropriate means of pedestrian access and crossing, both along the landscaped area (i.e., in a parallel direction with the property line) and across the buffer (i.e., providing pedestrian access to the site, separate from vehicular access points). Buffers and screens shall provide for appropriate hard-surfaced pedestrian access points and walkways where property lines abut existing or planned public streets, whether or not such streets have been constructed.
- Parking Lot Lighting – Supplemental parking lot lighting not provided by adjacent streetlighting shall be accomplished with high-cut-off fixtures that restrict light spillage beyond the limits of the parking area, using luminaires mounted no higher than 20 feet above grade and using posts and fixtures that are simple, contemporary "box" type fixtures with a black or dark finish.

C. EXTERIOR AND SITE LIGHTING

The following standards shall apply to exterior and site lighting:

- Fixture types for walkways – Lighting fixtures for walkways will be of a high cut-off type and the luminaires shall be bollard type or mounted no higher than 12 feet above grade.
- Prohibition on building illumination – The exterior facades of buildings may not be illuminated indirectly, except for the retail or restaurant space sited adjacent to Washington Street and the waterfront park.
- High cut-off fixtures – High cut-off fixtures are required for all other exterior and site lighting and may not be mounted at a height greater than 20 feet above grade.
- Light post bases – Light post bases, if not set in sidewalk paving, shall be at least 6 inches above the adjacent grade.

D. STREETS, DRIVES AND LOADING AREAS

All private streets, drives and loading areas will be paved with asphalt and provided with granite or concrete curbing. All streets will comply with the standard for public ways in Section VI. Design Standards for Public Ways, except for sidewalk dimensions.

V. RIVERFRONT PUBLIC OPEN SPACE DESIGN STANDARDS

The following guidelines address the public open space that will be provided through the development agreements and as may subsequently be improved by and for the City of Dover. These guidelines are distinct from the program for the public open space, which describes what will be designed - the content of the open space and park environment that will be created. The program will be separately documented. Instead, these guidelines provide directions regarding how the open space will be designed, by providing aesthetic and practical standards.

A. RIVERFRONT PUBLIC OPEN SPACE CHARACTER

The character of the waterfront park is intended to be urban relative to the natural landscape that occurs both upriver and downriver from this site. This implies the use of regularly spaced and consistent use of some standardized elements such as lighting fixtures, railings, and paving materials. It is not meant to imply a particular geometry or style of landscape and park design. However, the park character should be demonstrably related to relevant urban riverfront park precedents as reviewed and approved by those responsible for the design of a first class riverfront park.

In keeping with the mission of establishing and maintaining an active urban riverfront park, the introduction of public or private cafe style dining uses is encouraged. Such private seating and serving facilities occurring on City property must be licensed by the City Manager through the City Council. These cafe style spaces may be affiliated with restaurants located within the commercial spaces located along the street which fronts onto this public parkland space. The ideal location for such facilities would be at the wide plaza space adjacent to the proposed public marine facilities also referred to as the "node". It is intended that the primary cooking facilities and related restaurant infrastructure is located outside the parkland. Spaces to accommodate additional sidewalk food and drink vendors, licensed as temporary daily installations are also encouraged. It is envisioned that these sidewalk vending carts will be removed from the public property at the end of every day and will not become seasonal fixtures within the park.

Full implementation of interconnected waterfront festival uses would be consistent with the spirit and intent of this space. The design of this parkland and connections to Henry Law Park should anticipate such uses and activities.

There are three distinct sub-areas that have been defined which should have different characteristics. Figure 6 indicates the approximately location and extent of each of these areas.

The public open space along River Street and extending north of Washington Street along the edge of the planned restaurant or other riverside use should be consistent and provide continuity with the existing improvements in Henry Law Park. The design of railings, paving, and park furniture should either be the same or similar to that Park.

The public open space that extends northwards to the "node" at the River's bend should be designed as a public park, generally formal in character and reflecting the urban qualities of the downtown. This area should be adaptable to a range of uses and should include both "softscape" and "hardscape" elements.

Figure 6 - Open Space Segments



★ = VISUAL OR ACTIVITY NODE

The segment of the public open space that extends easterly from the “node” at the River’s bend is intended to be a public pathway that links the other public open spaces to the amenities and possible future uses along the River’s edge.

B. RIVERFRONT PUBLIC OPEN SPACE DESIGN ELEMENTS

1. Open Spaces

The park-like open space in the central portions of the public open space must include adequate lawn areas to accommodate casual public use as well as special events, festivals and the like. In addition, paved areas should be provided. Durable, hard surfaces should be incorporated where outdoor furniture including movable chairs and tables can be appropriately installed or used. Grading should provide for level lawn areas adjacent to the principal pedestrian paths to accommodate potential future booths, carts or temporary structures for special vending or events. The site areas to be provided and types of events to be accommodated will be defined by those responsible as part of the public open space program.

2. Land Forms and Structures

The land forms within the public open space should be simple and have limited portions of steep slopes, to the greatest extent practical. Public open space structures should be designed meeting the requirements of *Section IV - Private Sector Development Design Standards*. Direct access to the water’s edge is encouraged. However, where access to the water directly from the shore is provided, consideration of public safety should inform the design to avoid unsafe, slippery or hazardous conditions during all tidal and seasonal cycles.

3. Pedestrian Site Circulation

Pedestrian ways must be designed to be above normal high water. All paving materials that are within the 100-year flood plain must be designed to withstand flood conditions. Pedestrian circulation must be provided to transition ramps to riverfront floats at the water level that will be available on a seasonal basis for boating and direct access for pedestrians.

4. Water Access

The design of the public landing shall be of durable materials that will withstand tidal action, flooding and winter conditions. To the extent that ADA applies to such marine facilities, these regulations must be considered and applied. The standards below are intended to provide guidance with respect to the nature of any proposed waterfront access facilities.

The design of the public landing facilities should include floating concrete docks connected to a guide pile system designed to receive public tour boats and ferry boats. The concrete flotation and guide pile system should be designed to remain in place for all seasons and all anticipated river conditions. There should be a minimum of 60 linear feet of berthing surface with a minimum dock width of 10 feet. The designed freeboard height should accommodate passenger vessels. The public access gangway should be of

aluminum or of a material with similar durability and maintenance free characteristics. The gangway system should be of sufficient width to allow for concurrent, two way pedestrian travel. The gangway should be designed to remain in place in all seasons with the option of retracting the gangway during winter months. The materials specified for the piles, floats and gangway must have a design life of 25 years in the conditions.

To accommodate recreational pleasure craft, adjacent and connected to the main public landing dock, additional fingers are encouraged. These facilities should be designed for the temporary docking of light water craft vessels of less than 25 feet in length down to and including personal watercraft such as canoes and kayaks. No facilities are planned or intended for long term docking or storage of any size vessel other than a vessel of significant historic value as to be determined to be an attraction by the City Manager and City Council. No boat ramp and access drive shall be located within the public parkland. It is assumed that other public and/or private facilities will be used for launching vessels.

5. Gateways

Landscape or architectural features should be used to indicate the major transitions and provide visual cues between the riverfront public open spaces to the pedestrian pathways and the sidewalk network to which it is connected.

6. Paving Materials

Acceptable paving materials include concrete, brick, or concrete unit pavers. At least 10 percent of the paved areas shall be composed of brick or unit pavers to serve as accent and create interest at gateways or other portions of the public open space. Asphalt surfaces are permitted only for parking or loading areas.

7. Riverside Boardwalk or Sidewalk

Where a riverside boardwalk or sidewalk is provided, it shall be constructed of cast in place concrete or manufactured planking materials (boardwalk). Boardwalk elements will be anchored with structurally designed pilings or foundations, and provide for well-founded transitions with other paving materials and structures to provide an even walking surface. Boardwalk elements must be above normal high water lines, and be designed to withstand flood conditions if below 100-year flood plain elevations.

8. Pedestrian Character

This aforementioned table may be extended along the street that lies in front of the proposed retail units from Washington Street to the "node". This special paving surface and bollard treatment of the street is intended to create a more pedestrian feel for the street and to facilitate street fair type functions. The design of this space should strive to visually connect and merge the parkland with the storefronts on the opposite side of the street using durable pavers and bollards. To further identify the parking edge and to facilitate surface drainage, the vertical curbing on the storefront side of the street should not be eliminated except for limited locations of handicap cross walks as depicted in Figure 1 on the preceding pages.

9. Lighting

The lighting fixtures within the public open space will be as described under Section II, Shared Design Standards, Lighting. Supplemental fixtures, if provided, will be of a bollard-type with high-cutoff characteristics to limit light spillage onto water surfaces. Luminaires shall be energy-efficient.

10. Plantings

The public open space plantings should be designed so that, at maturity, the plantings will not obscure visibility of any portion of the public open space from public ways, adjacent private residences, or other uses.

11. Site Furniture

The site furniture including railings, benches and other elements including bicycle racks will be the same or compatible with those employed within Riverwalk in Henry Law Park. All site furniture will be of similar style and be consistent in terms of quality and appearance. Where seating areas are provided, the design of special walls or other adaptable features of the public open space may be employed in place of benches.

12. Signage

Signage indicating the name of the public open space should be provided at each gateway location. In addition, signage should provide wayfinding information for any extensions of the riverwalk that may have been created to the north of the Development.

13. Parking

Parking within the riverfront public open space is restricted to parallel parking, angled parking, or perpendicular parking along streets; no parking lots will be permitted.

14. Public Art

The design of the public open space should be coordinated with any funded art installations being provided contemporaneously with the initial construction of the park.

15. Public Restrooms

The design should accommodate public restrooms in a location within or adjacent to the open space. The design and operational assumptions should be confirmed by the City. If restrooms must be phased due to funding constraints, then a future location should be determined and designated.

C. RIVERFRONT PUBLIC OPEN SPACE OPERATIONAL AND MAINTENANCE

1. Flood Conditions and Durability

For all installations and improvements that are within the 100-year flood plain, materials shall be specifically designed to be durable and anchored in a manner intended to withstand occasional flood conditions. All electrical connections, conduit, outlets shall be located outside of the flood plain, or designed for immersion and flooding conditions.

VI. DESIGN STANDARDS FOR PUBLIC WAYS

A. STREETS

1. Dimensions

Streets widths should be as narrow as is consistent with sound engineering to preserve the sense of open space and pedestrian orientation. Notwithstanding Dover Subdivision Regulations Section 155-33, or any other applicable local, state or federal standards, street design is encouraged to conform to the following:

- Lane widths – Lane widths of streets and ways other than along Washington Street should be no greater than 11 feet. Lanes along Washington Street should be no greater than 12 feet.
- Turning radii – Turning radii should be no greater than that required by the City’s officials with jurisdiction.

These width design standards are integral to the general character of the Waterfront community encouraged to be designed by these Dover Waterfront *Design Guidelines*. They are written acknowledging the need for road width waivers pursuant to Dover Subdivision Regulations Section 155-51 and in contemplation that such waivers should be granted to achieve the development goals for this unique waterfront property.

2. Crosswalks

Crosswalks will be marked by painted markings or imprinted, colored asphalt using a pattern and color that is distinct from the adjacent sidewalk, unless composed of a raised table condition as noted below.

- Raised table – A raised table crosswalk condition may be required at the discretion of the City at intersections that provide direct connections to the riverfront public open space. Raised tables shall be continuous with the adjacent sidewalks, and be separated from the sidewalks with bollards. Raised table paving will be imprinted, colored asphalt using a pattern and color that is distinct from the adjacent sidewalk.

3. Curbing

Granite curbing will be provided along all public streets.

B. SIDEWALKS

Sidewalks will be at least 8 feet wide except within public open spaces as indicated within these *Design Guidelines*. Sidewalks shall be constructed of concrete with at least 5% of the paving area provided in brick or concrete unit pavers to provide accents.

C. STREETScape AND LANDSCAPING

Street trees, if located within sidewalk areas, will be accommodated either with tree grates at least 4 feet by 4 feet in dimension, or within raised planting areas at least 4 feet by 8 feet in dimension, including a 5” granite curb raising the planted surface above the adjacent sidewalk.

D. ON-STREET PARKING

On street parking may be provided using parallel, perpendicular or diagonal parking meeting standard engineering practices in terms of minimum dimensions.

E. SIGNAGE

1. Public Amenity Identification and Wayfinding Signs

Signage should be provided at the intersection of Washington Street and the main interior street indicating the direction to the riverfront public open space and other public amenities that may be provided. Adequate space should be provided adjacent to this sign for temporary signs that may be provided to support special events, festivals and the like. This signage should be lighted through indirect lighting from street lighting or other sources.

2. Street Signs

Street signs should be provided naming all intersecting streets and use the same materials, size and graphics as standard Dover signage.

3. Parking Identification Signs

Parking identification signs should indicate public parking spaces and any associated restriction; these should be of a uniform size, color and symbol and be mounted on low posts no higher than 4 feet.

APPENDIX A: GLOSSARY OF TERMS

The following glossary defines terms as they are specifically employed with the Design Guidelines. Other definitions of these terms may be associated with other applicable regulations or requirements.

- Floor – An occupied floor for any use. Partial floors (1/2 floors) will be calculated as occupying a net floor area that is no greater than 50 percent of the net floor area of the floor immediately below.
- Main Interior Street – This is the primary interior street that serves as the publicly-owned vehicular circulation link between Washington Street and the riverfront park and open space.
- Primary Façade Materials – The predominate façade materials are those that are employed to face at least 80 percent of the façade, excluding door and window frames and glazing, ornament and trim, and mechanical covers, grilles, canopies or awnings.
- Riverfront Promenade – This is the primary upland walkway that will provide continuous public access along or within the public riverfront park.
- Street - A Street is a vehicular route within the Cochecho Waterfront Development that is intended for shared use including public access, regardless of its ownership. This definition does not include interior circulation required within a parking lot, or accessways to loading or parking areas that will be devoted to residents and/or business and commercial tenants, guests and invitees only because of their location and configuration.
- Style – The exterior architectural character of a building as expressed in the integrity among the form, materials and exterior detail relative to recognized compositional types. The book *Identifying American Architecture* (John J.-G. Blumenson, 1981) will serve as the reference source for identifying architectural styles. These guidelines recognize that there are many variations in the application of style and stylistic elements, and that innovative approaches to traditional architectural styles may be appropriate.

APPENDIX B: RELATIONSHIP TO EXISTING REGULATIONS AND DOCUMENTS

The *Design Guidelines* advance defined public interests and purposes that are directly related to the design of buildings, landscape, street and sidewalks, infrastructure and signage within the Cochecho Waterfront Development. The following discussion provides lists the relevant documents that provide the criteria used to assemble the *Design Guidelines*, each of which has been established through public processes.

1. City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter

This document establishes a policy basis for land use decisions and regulations including relevant aspects of the Cochecho Waterfront Development. There are applicable policies associated with the three different component areas addressed within the LDA and the Cochecho Waterfront Development: private sector redevelopment, public rights-of-way, and public open space. The list of applicable policies is included in *Appendix B - Summary of Applicable Public Criteria, Standards and Regulations*.

2. City of Dover Zoning Code

The City of Dover Zoning Code includes numerous provisions that provide guidance regarding the design elements that are within the scope of these *Design Guidelines*. These *Design Guidelines* employ relevant portions of the Zoning Code as the basis for site-specific standards for the Cochecho Waterfront Development. The *Design Guidelines* are intended to be consistent with the purposes of the *Zoning Code* and the regulations that are applicable to the site. An outline of the relevant sections of the *Zoning Code* is included in *Appendix B - Summary of Applicable Public Criteria, Standards and Regulations*.

3. Dover Waterfront Design Task Group Survey Results

The CWDAC Design Task Group prepared a series of recommendations regarding the design of the Cochecho Waterfront Development. These were documented as the *Dover Waterfront Design Task Group Survey Results*. These recommendations were subsequently endorsed by the CWDAC and established as its policy criteria for various aspects to the future projects. Some of the recommendations are associated with the program of uses, operations and management that are not within the scope of these *Design Guidelines*. The applicable recommendations reflect the roles and responsibilities for design decisions associated with each of the three different component areas addressed within the LDA and the Cochecho Waterfront Development: private sector redevelopment, public rights-of-way, and public open space. An organized list of the relevant criteria from the *Dover Waterfront Design Task Group Survey Results* is included in *Appendix B - Summary of Applicable Public Criteria, Standards and Regulations*.

4. Cochecho Waterfront Design Charrette 2005 Version

As part of the public planning process for the Cochecho Waterfront Development, CWDAC sponsored a planning forum in 2005. The results are documented in the Cochecho Waterfront Design Charrette 2005 Version. The conclusions of that forum

regarding architecture and site design were specifically included in the adopted *Dover Waterfront Design Task Group Survey Results* by reference. Various of the criteria apply to each of the three component areas of the Cochecho Waterfront Development: private sector redevelopment, public rights-of-way, and public open space. The list of applicable criteria are contained in *Appendix B - Summary of Applicable Public Criteria, Standards and Regulations*.

APPENDIX C: RELATED PUBLIC POLICIES, CRITERIA AND REGULATIONS

As noted in the initial section of this document, The *Design Guidelines* advance defined public interests and purposes that are directly related to the design of buildings, landscape, street and sidewalks, infrastructure and signage within the Cochecho Waterfront Development. This appendix lists all of the applicable aspects of plans and policies that have been established through various public processes that are reflected in the *Design Guidelines*. It may be used as a reference relative to the public purposes that are implemented through the *Design Guidelines*.

1. City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter

This document establishes a policy basis for land use decisions and regulations including relevant aspects of the Cochecho Waterfront Development. As a public policy document, the relevant criteria are applicable to all three different component areas addressed within the LDA: private sector redevelopment, public rights-of-way, and public open space.

The applicable recommendations have been organized to reflect the roles and responsibilities for design decisions associated with the three different component areas addressed within the LDA: private sector redevelopment, public rights-of-way, and public open space. Relevant portions that have served as a reference in establishing these *Design Guidelines* include the following excerpted components from the Vision, Goals and Objectives:

Vision Statement

A City with an emerging urban vibrancy guided by a small town sense of community.

Goals and Objectives

- I. To be an attractive place where people live, work, and recreate.
- IV. To promote a vibrant downtown with diverse uses.
- VI. To create and maintain a safe, clean and aesthetically pleasing natural and manmade environment.
- VIII. To develop and redevelop land respecting the surrounding context and the environment.
- IX. To encourage sustainable growth that is sensitive to environmental issues and minimizes energy consumption.

(City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter, page 7)

The following recommendations from the *2007 Update to the Land Use Analysis Chapter* are also specifically applicable and have been employed as a reference for these *Design Guidelines*.

Streetscape

Public - Downtown

S4 – Commercial streets shall have shade trees planted every 30 feet at a minimum height of 15 feet. Spaced with party walls of buildings, may be optional in the presence of conflicting awnings.

S5 – Concrete or brick sidewalks, no asphalt. Tree wells should always be brick to promote root health.

S6 – Streetlights, mailboxes, trash receptacles and other obstructions are placed within the tree strip.

S7 – Benches face each other within the tree strip or are backed up to buildings.

S8 – Transformers, HVAC equipment, ventilation and other machinery are discouraged from the streetscape.

S9 – Streetlights are low height and wattage and appear frequently toward neighborhood centers (approximately every 30 feet) and less frequently toward rural areas.

S10 – Identify strategic locations for landscaping improvements along corridors to improve aesthetics.

Other

Natural

N2 – Create public spaces and thoroughfares that are at least partially fronted by significant natural amenities.

N4 – Minimize grading to the amount necessary for safe development.

N5 – Connect natural spaces through continuous corridors, through neighborhoods or through narrow green belts.

N6 - Identify strategic locations for landscaping improvements along corridors to improve aesthetics...

Energy

E3 – Promote sustainable development through use of recycled materials, Energy Star rated products and sustainable building materials.

(City of Dover, New Hampshire Master Plan, 2007 Update to the Land Use Analysis Chapter, pages 49-51)

2. City of Dover Zoning Code

The City of Dover Zoning Code includes numerous provisions that provide guidance regarding the design elements that are within the scope of these *Design Guidelines*. These

Design Guidelines employ relevant portions of the Zoning Code as the basis for site-specific standards for the Cochecho Waterfront Development. The *Design Guidelines* are intended to be consistent with the purposes of the Zoning Code and the regulations that are applicable to the site.

In this regard, the zoning standards are not formally applicable to the City’s own projects and property when employed for public use such as roadways, sidewalks and parks. As a result, the relevant criteria are applicable only to the private sector redevelopment.

The following list refers to the applicable portions of the Zoning Code that provide additional public policy framework and purposes associated with these *Design Guidelines*:

Criteria from the Site Review Regulations. Ch. 149	
Zoning Section/ Sub-section	Notes on Criteria/ Requirement
149-14 Site Development Design Criteria	
149-14.B Utilities	
149-14.B.3(a) Utilities Electric/ Cable	Required to be located underground for new installation.
149-14.L.8(d) Utilities, Other ground level mechanicals	To the extent practical, all utility elements, such as dumpsters, utility meters, and ground mounted air conditioning units, should be screened and located such that they are not visible from a public way or neighboring residential properties. Also emphasized in 149-14.G.3(b)
149-14.C Driveways, etc.	
149-14.C(1) Driveways, access-ways, internal roads	Required to have sidewalks, storm water drainage, pavement, gutters.
149-14.D Parking Areas	
149-14.D(1) Parking	No on-street parking is allowed.
149-14.D(3)(a) Parking Lot Design	90 degree angle: 9’ wide X 18’ long 60 degree angle: 9’ wide X 21’ long
149-14.E.2 (b, c) Outdoor Lighting Parking Lot Lighting	Mounting Height: Max 20’ Minimum and maximum illumination level: .3 and .5 foot candles. Planning Board may permit substitution of architectural lighting if it is appropriate at a maximum height of 15’.
149-14.G Parking Lot Landscaping 149-14.G.1(a, b, c) Interior Landscaping	20 or fewer spaces: none 25-125 spaces: 5% 126-375 spaces: 8% >375 spaces: 10% Specifications are provided for landscaped planting strips, islands and perimeter buffers as well. Includes size of planting, type, and location.

Zoning Section/ Sub-section	Notes on Criteria/ Requirement
149-14.G.2(a, b) Parking Lot Landscaping	<p>Lots with 10 or more spaces will have a perimeter planting of deciduous trees planted 40 ft. on center w/in the buffer area required (see 149-15.C). Additional specifications are included for species, height, caliper, etc.</p> <p>Planning Board can allow alternative methods for parking lot screening if applicant can demonstrate adequate screening is achieved. Additional screening methods area encouraged include earthen berms planted with grass with shrubs, fencing, and low walls.</p>
149-14.J Interconnection Between Parcels	
149-14.J(1)	“In order to reduce the number of vehicles entering and exiting arterial and collector roads, the provision of interconnecting driveways is required whenever feasible.”
149-14.J(3)	The Planning Board can vary the standard in recognition of unique circumstances regarding a parcel of land.
149-14.K Location of Parking Lots	
149-14.K(1)	Buildings should be sited closer to the street and parking should be located to the side or rear of the building.
149-14.K(2)	Planning Board may waive requirements in recognition of unique circumstances.
149-14.L Architectural <i>Design Guidelines</i>	
149-14.L.5: Sections of the City 149-14.L.5(b)	“...this zone is adjacent to the existing downtown core, and should feel as if it is an extension of the B-2, UMUD district. The architecture should encourage marine, history or tourism related development.”
149-14.L.6(a)	“No particular architectural style is stipulated and innovative, contemporary, and distinctive designs are encouraged, provided they are respectful of general design principles and context.”
149-14.L.6(b)	“Buildings should be compatible with traditional New England architecture.”
149-14.L.7(a) Siting	To the extent practical, buildings should be located parallel or perpendicular to the street and not at odd angles unless dictated by strong site characteristics.
149-14.L.7(b) Scale	“Every effort should be made to provide an appropriate scale to new buildings both in their overall size and in their details. If practical, it is preferred that buildings contain at least two stories. Alternatively, a single story building should have a relatively steep roof or a high parapet.”
149-14.L.7(c) Proportion	“Buildings and their details should be well proportioned in accordance with commonly accepted design principles so as to create a sense of order and balance.”
149-14.L.7(d) Massing	Large buildings should be broken into smaller masses to provide a human scale to development. The smaller parts should relate to each other and have their own design integrity.

Zoning Section/ Sub-section	Notes on Criteria/ Requirement
149-14.L.7(e) Roofs	<p>“Incorporation of a moderate slope is preferred. Where flat roofs are used, however, there should be a distinct cornice and/or parapet to emphasize the top of the building. Extensive areas of visible roof should be broken up with dormers, cross gables, cupolas, chimneys, parapets, balustrades, and towers.”</p> <p>“The roofs of all buildings located on or near the line of a street shall be protected by railing or guards of sufficient height and strength to prevent the falling of ice and snow from such roofs to the sidewalks of such streets. The owner of such a building shall be responsible for the adequacy of such railings or guards.”</p>
149-14.L.7(f)	“...blank walls are inappropriate. Traditionally, the parts of a facade that might be embellished, or at least articulated in some fashion (may) include: foundation, cornice, belt course, portico and window surrounds.”
149-14.L.7(g)	“...use of large picture type windows for retail uses on the first floor is strongly encouraged. It is preferable that windows be vertical...Horizontally shaped windows are discouraged.”
149-14.L.7(h)	“Articulation of the entrance is encouraged through use of a portico, canopy, awning, sidelights, surround, or other device. Generally, there should be an entrance, if not the primary entrance, located on the front facade.”
149-14.L.7(i)	“The use of natural materials or materials that appear natural is preferred. Materials should be high quality and durable. Wood (clapboard and shakes), brick, stone, fiber reinforced stucco, textured block, and terra cotta are the preferred materials, although fabricated materials which effectively imitate the character of these materials is acceptable. Conventional vinyl and aluminum siding arranged in a horizontal clapboard pattern is acceptable but not preferred.”
149-14.L.7(j)	“Color of buildings is reviewed for nonresidential property only. Generally, it is preferable to use two or three colors. The main color(s) on a building should generally be nature blending, earth tone, neutral, or pastel in character. Bright colors should be limited to accent areas. High intensity colors, metallic colors, or fluorescent colors should not be used.”
149-14.L.7(k)	“Use of low key, low intensity wall pack or spot type lighting, or lighting of signage on buildings is appropriate. Use of lighting to highlight the building in a prominent manner, such as brightly illuminated roof fins or neon tube lighting is discouraged.”
149-14.E.1 Outdoor Lighting	In commercial areas, the principal building shall be lighted on all sides.
152 Streets and Sidewalks	
152-14 Article VIII, Marquees, Signs and Awnings.	“...the lowest part of such sign or awning shall be at least eight (8) feet above the sidewalk, and lowest of any marquee shall be at least ten (10) feet above the sidewalk.”

Zoning Section/ Sub-section	Notes on Criteria/ Requirement
170, Article V, Dimensional Regulations	
170-16 Table of Dimensional Requirements: CWD District	<p>A maximum height 55' allowed.</p> <p>Up to 100% lot coverage is allowed on lots located in CWD and South of the Cochecho River.</p> <p>Up to 75% coverage is allowed along the CWD R-12 boundary. Buildings must be setback 75' from said boundary and paved areas setback 25'.</p>
170 Article IX, Signs and Fences	
170-32 Sign Review and Regulation Table	Two signs allowed per use. An additional one (1) square foot of sign area per one (1) foot of business frontage shall be permitted, provided that the character of the sign conforms to mill motif design criteria.
170-32.P(1)	Within the defined Mill Motif theme, a great amount of flexibility is permitted where design and materials are involved, and all signage should be guided by general design criteria.
170-32.F Billboards	No billboards are permitted
170-32.D(20) Awnings	Awnings may be used in lieu of permitted wall signs provided text doesn't exceed allowed sq. footage for a wall sign.
170-32 Other Requirements	<p>Signs shall be located with respect to the basic architectural framework of the building, so as not to obscure the primary elements (door and window openings and decorative facade treatments) of a building's framework (graphic provided).</p> <p>Very specific sign composition guidelines such as lettering, color, etc. are provided.</p>

3. Dover Waterfront Design Task Group Survey Results

The CWDAC Design Task Group prepared a series of recommendations regarding the design of the Cochecho Waterfront Development. These were documented as the Dover Waterfront Design Task Group Survey Results. These recommendations were subsequently endorsed by the CWDAC and established as its policy criteria. Some of the recommendations are associated with the program of uses, operations and management that are not within the scope of these *Design Guidelines*.

The recommendations that are associated with design characteristics are provided below. The applicable recommendations have been organized to reflect the roles and responsibilities for design decisions associated with the three different component areas addressed within the LDA: private sector redevelopment, public rights-of-way, and public open space. In cases where the same criteria apply to multiple components, the criteria are repeated within each relevant section.

3.1 Private Sector Redevelopment

The following is the list of Dover Waterfront Design Task Group Survey Results criteria that are associated with required standards within the *Design Guidelines*:

- A mix of building materials – brick, wood, glass – should be used.
- Architecture and building design should be less sterile/generic. (*note: this is interpreted as a recommendation relative to the building design representations before the CWDAC at the time that the recommendations were drafted*).
- Architecture should complement, but not be identical to, current downtown style.
- Natural materials should be used.
- Architecture should reflect the community vision as defined in City’s 2005 design charrette (*note: see the related list of criteria below*).
- Different architectural styles and designs should be used to create variation between buildings.
- Architecture should reflect Dover’s history.
- The site layout and design (including buildings, parks, roads, walks) should follow the contour of river.
- The site plan should reflect the priorities established in the City’s 2005 design charrette (*note: within the scope of these Design Guidelines, this is interpreted to mean that those recommendations of the Cochecho Waterfront Design Charrette 2005 Version that specifically related to site design are adopted by reference. The relevant recommendations are separately listed in the section below*).
- The site layout should have an “urban” feel.
- The Waterfront should feature smaller plots, or large plots that can be subdivided and developed separately in phases.
- Buildings should allow for townhouse style vertical ownership
- Buildings should be sited and designed to be adaptable for both commercial and residential use
- Building interiors should be flexible and easily convertible from residential to retail or gallery use

The following is the list of *Dover Waterfront Design Task Group Survey Results* provided for the consideration of the designated developer relative to the private redevelopment; as these were advisory recommendations, they have not been incorporated into the *Design Guidelines* unless other public policy standards that establish the same criteria also apply:

- Architecture should not be neo-Victorian, as currently presented.
- A mix of old and new architectural styles should be used.
- Architecture should have an “urban” feel.
- Brick and granite building materials should be used.

- Architecture should be consistent with the existing downtown style.
- Architecture should reflect Dover’s waterfront history (e.g., wood clapboard, shingles).
- The site layout and design should reflect Dover’s waterfront history.
- The buildings should all be much smaller.
- The buildings should be closer to river.
- The flood plain along the eastern edge of the property should not be developed.
- All building first floor units should be at ground level.
- The developed Waterfront should be visually attractive when viewed from the river.

3.2 Public Open Space

The following is the list of criteria that are applicable to the public open space within the Cochecho Waterfront Development and that are reflected in the *Design Guidelines*. Some of these criteria were listed in the original document as recommendations for consideration by the designated developer. However, the design of the improvements within the public rights-of-way will be entirely controlled by the City, and will not be dependent upon the decisions or preferences of the designated developer. It should be noted, as a consequence, that these criteria are included in the *Design Guidelines* for public open space but are subject to other City actions, funding availability and other decisions that may be made by the City. The City will be also separately responsible for establishing the program of uses that will be provided within the public open spaces; the use recommendations from the Survey Results are not included in the list of design-related recommendations.

- The riverwalk should be more welcoming (*note: this is interpreted to mean that the design should be enhanced relative to the site plans that were the subject of the Survey Results*).
- The riverwalk should clearly connect Henry Law and Maglaras Parks.
- The riverwalk/park area should feature more opportunities to interact with the water (*note: this is interpreted to mean that the design should be enhanced relative to the site plans that were the subject of the Dover Waterfront Design Task Group Survey Results*).
- The riverwalk should include looping paths.
- A focal point should be added at the end of the promenade.
- A boardwalk should be added along water’s edge.
- The site layout and design (including buildings, parks, roads, walks) should follow the contour of river.
- Hardscaped seasonal retail kiosks should be included in the public area (*note: this is interpreted to mean that the design should be able to accommodate seasonal retail kiosks that would be provided by seasonal tenants or for special events; the provision of*

such kiosks is assumed to be addressed through the management and operations of the open spaces).

- Designated activity areas should be added along the riverwalk to create vitality
- Sculpture should be included within the park area (*note: in the context of the Design Guidelines, this is interpreted to mean that the design should be able to accommodate public sculpture*).
- A hardscaped festival area should be included in the public space.
- The developed Waterfront should be visually attractive when viewed from the river.
- The Waterfront should connect people to the river.
- The Waterfront should provide opportunities for the public to safely touch the water. Waterfront lighting should complement existing downtown styles
- Roadways from Waterfront site should connect through Maglaras Park. (*note: this is interpreted to mean that the design and layout of the internal publicly-accessible roadway network should provide for possible future connections to Maglaras Park*).
- The Waterfront should include more pedestrian streets and zones. (*note: this is interpreted as a recommendation relative to the site plan proposal before the CWDAC at the time that the recommendations were drafted*).
- Roadways from Waterfront site should connect to Henry Law Ave.

3.3 Public Rights-of-way

The following is the list of Dover Waterfront Design Task Group Survey Results (Survey Results) that are applicable to the public rights-of-way within the Cochecho Waterfront Development and that are reflected in the *Design Guidelines*. Some of these criteria were listed in the original document as recommendations for consideration by the designated developer. However, the design of the improvements within the public rights-of-way will be entirely controlled by the City, and will not be dependent upon the decisions or preferences of the designated developer. It should be noted, as a consequence, that these criteria are included in the *Design Guidelines* for public rights-of-way but are subject to other City actions, funding and decisions.

- The site layout and design (including buildings, parks, roads, and walks) should follow the contour of river.
- The Waterfront should include more pedestrian streets and zones. (*note: this is interpreted as a recommendation relative to the site plan proposal before the CWDAC at the time that the recommendations were drafted*).
- The Waterfront should include a trolley stop from the train station to waterfront.

4. Cochecho Waterfront Design Charrette 2005 Version

As part of the public planning process for the Cochecho Waterfront Development, CWDAC sponsored a planning forum in 2005. The results are documented in the *Cochecho Waterfront Design Charrette 2005 Version* (the “*Charrette 2005 Version*”). The particular

conclusions of the *Charrette 2005 Version* regarding architecture and site design were specifically included in the CWDAC-endorsed *Survey Results* by reference, and are therefore applicable as criteria for the *Design Guidelines*. The list of applicable architecture and site design conclusions is divided among the three different component areas addressed within the LDA and Cochecho Waterfront Development: private sector redevelopment, public rights-of-way, and public open space.

4.1 Private Sector Redevelopment

Scale and Use

2. Ground floors of buildings should be considered for such active uses as restaurants, shops and other retail uses, especially when adjacent to public spaces.

3. Buildings should be a mixture of two to four stories. Buildings may share a common wall and should be staggered with areas of public access located between and/or around them.

4. Density pattern of new development should complement current adjacent development and the unique historic waterfront environment.

5. Incorporate a mixture of building types, footprints, architectural forms, materials and colors that are compatible with existing downtown built forms and reflect Dover's architectural heritage.

8. All lighting and fences utilized within the site and along walkways shall be consistent with the style and height as installed along the existing Riverwalk in Henry Law Park.

9. The front of buildings located adjacent to the river shall be no closer than 35 feet from the shoreline and no further than 100 feet unless the building is for a waterdependent use. Building heights can vary and may increase as the building setback increases. Structures should have variable setbacks so as to create a staggered wall appearance and to provide spaces for public enjoyment and outdoor activities related to the structure. Greater or lesser setbacks will be considered depending on the use and provision for public access. The following provides a general standard for building placement along the waterfront:

- Setback from shoreline (water-dependent only) 15 ft. Max. height 0 ft.
- Setback from shoreline 35 ft. Max. height 35 ft. (2 1/2 stories)
- Setback from shoreline 50 ft. Max. height 40 ft. (3 stories)
- Setback from shoreline 75-100 ft. Max. height 45 ft. (3 1/2 stories)

10. Coverage, building height and footprint requirements may be modified in specific instances as long as the overall development is consistent with goals and other *Design Guidelines* outlined in this charrette document.

11. Locate new structures, landscape features or other uses so as to encourage opportunities for public use of the river from nearby public roads or spaces and upland area.

12. The layout of structures within the site shall preserve views to the river to the greatest extent possible.
13. Length of contiguous structures shall be planned so as to establish and protect view corridors to the waterfront and to preserve pedestrian access routes from upland areas to the waterfront.
14. All new development shall be integrated into the existing landscape where applicable so as to minimize its visual impact and enhance shoreline areas through use of vegetative and structural screening, landscaping and grading.
15. Within the developable portions of the waterfront, there should be an appropriate intermingling of structures and impervious surfaces and landscaping.
16. Signage shall conform to the regulations outlined in Chapter 170, Zoning, subsection 170-32 (F) and, in general, be of historical or mill motif character as defined in the ordinance.

Environmental Quality

1. Recognize the transition from a “rural” river environment to an urban waterfront.
2. New landscaping should include strategic placement of native, non-invasive species to ensure that new development maintains and enhances the visual quality of the shoreline and overall waterfront site.

Circulation and Access

1. Incorporate a waterfront walkway of variable character in an area between the shoreline and building frontage. Where other uses may encroach upon this walkway, ensure provision of continuous pedestrian circulation along the waterfront.
5. Walkways shall be of natural materials and consistent with the surrounding downtown (such as brick, cobblestones, granite, etc.) to the greatest extent possible.
6. All pedestrian walkways shall be separated from roadways by landscaped areas, where possible, or granite curbing, at a minimum.
7. Link waterfront environment with upland areas by establishing pedestrian routes.
8. Off-street parking shall: (1) be consistent with the City of Dover Zoning Ordinance; (2) may be provided off-site within a reasonable walking distance; (3) may be provided by the creation of a parking facility; and (4) incorporate appropriate landscape features to break up the expanse of pavement. (See the Framingham, MA, Highway Overlay District Regulations section of Site Plan Review Regulations).
9. Internal roadways should be designed in a curvilinear fashion in order to take advantage of existing topography and encourage low vehicle speeds.
10. Internal roadway pavement should not exceed 24’ in width and shall maintain a parkway appearance.

4.2 Public Open Space

Scale and Use

8. All lighting and fences utilized within the site and along walkways shall be consistent with the style and height as installed along the existing Riverwalk in Henry Law Park.

11. Locate new structures, landscape features or other uses so as to encourage opportunities for public use of the river from nearby public roads or spaces and upland area.

Environmental Quality

1. Recognize the transition from a “rural” river environment to an urban waterfront.

2. New landscaping should include strategic placement of native, non-invasive species to ensure that new development maintains and enhances the visual quality of the shoreline and overall waterfront site.

Circulation and Access

1. Incorporate a waterfront walkway of variable character in an area between the shoreline and building frontage. Where other uses may encroach upon this walkway, ensure provision of continuous pedestrian circulation along the waterfront.

3. Ensure that historic/interpretive signage motif is compatible with that established in the Henry Law/Cochecho Riverwalk project.

4. Ensure access to the river by activities such as lowering the seawall in certain locations and by providing public landing facilities for river excursion vessels, as well as tourist and recreational navigation.

5. Walkways shall be of natural materials and consistent with the surrounding downtown (such as brick, cobblestones, granite, etc.) to the greatest extent possible.

6. All pedestrian walkways shall be separated from roadways by landscaped areas, where possible, or granite curbing, at a minimum.

7. Link waterfront environment with upland areas by establishing pedestrian routes.

8. Off-street parking shall: (1) be consistent with the City of Dover Zoning Ordinance; (2) may be provided off-site within a reasonable walking distance; (3) may be provided by the creation of a parking facility; and (4) incorporate appropriate landscape features to break up the expanse of pavement. (See the Framingham, MA, Highway Overlay District Regulations section of Site Plan Review Regulations).

4.3 Public Rights-of-way

Scale and Use

8. All lighting and fences utilized within the site and along walkways shall be consistent with the style and height as installed along the existing Riverwalk in Henry Law Park.

11. Locate new structures, landscape features or other uses so as to encourage opportunities for public use of the river from nearby public roads or spaces and upland area.

Environmental Quality

2. New landscaping should include strategic placement of native, non-invasive species to ensure that new development maintains and enhances the visual quality of the shoreline and overall waterfront site.

Circulation and Access

5. Walkways shall be of natural materials and consistent with the surrounding downtown (such as brick, cobblestones, granite, etc.) to the greatest extent possible.

6. All pedestrian walkways shall be separated from roadways by landscaped areas, where possible, or granite curbing, at a minimum.

7. Link waterfront environment with upland areas by establishing pedestrian routes.

8. Off-street parking shall: (1) be consistent with the City of Dover Zoning Ordinance; (2) may be provided off-site within a reasonable walking distance; (3) may be provided by the creation of a parking facility; and (4) incorporate appropriate landscape features to break up the expanse of pavement. (See the Framingham, MA, Highway Overlay District Regulations section of Site Plan Review Regulations).

9. Internal roadways should be designed in a curvilinear fashion in order to take advantage of existing topography and encourage low vehicle speeds.

10. Internal roadway pavement should not exceed 24' in width and shall maintain a parkway appearance. No parking shall be allowed on the primary circulation roadway. *(note: this last sentence is interpreted to apply to the Washington Street extension as the primary circulation roadway).*

(Cochecho Waterfront Design Charrette 2005 Version, pages 15 to 21)

APPENDIX D: ARCHITECTURAL PRECEDENTS

CIVIC and COMMERCIAL ARCHITECTURE

	<i>Building Name</i>	<i>Address</i>	<i>Date of Construction</i>
1.	Friends Meeting House	141 Central Avenue	1768
2.	Hosea Sawyer Block	29-31 Main Street	1825
3.	Belknap School	65 Belknap Street	1856
4.	Orchard Street Firehouse	1 Orchard Street	1865
5.	Brick House	2 Orchard Street	–
6.	Morrill Block	468-484 Central Avenue	c. 1870
7.	St. John's Church	18 Chapel Street	1875
8.	Wyskiel, Boc & Tillinghast	561 Central Avenue	–
9.	Bracewell Block (1910)	422-432 Central Avenue	1879
10.	Newberry's/National Block	446 Central Avenue	c. 1880
11.	National Block Detail	436-454 Central Avenue	c. 1880
12.	Merchants Row I	364-386 Central Avenue	–
13.	Merchants Row II	388-400 Central Avenue	–
14.	Kelley's Row	421 Central Avenue	–
15.	Coheco Mill Fanlight	100 Main Street	1880
16.	Coheco Mill Tower	100 Main Street	1880
17.	Coheco Mill	100 Main Street	1880
18.	Coheco Mill & Bridge	100 Main Street	1880
19.	Walker Block	104 Washington Street	c. 1880
20.	St. Thomas Episcopal Church	5 Hale Street	1891
21.	Strafford County Courthouse	10 Second Street	1889
22.	Strafford Bank	83 Washington Street	1895
23.	King Block	Orchard Street & Central Avenue	c. 1900
24.	Woodman Block	276-286 Central Avenue	1895
25.	Woodman Block Storefront	3 Hale Street	1895
26.	Kennedy Building	505 Central Avenue	1902
27.	McConnell Center	61-65 Locust Street	1904
28.	Dover Public Library	73 Locust Street	1905
29.	U.S. Post Office	133 Washington Street	1909
30.	Franklin Block/Montgomery Ward	451 Central Avenue	c. 1900
31.	Café Ciabatta	2 Pierce Street	–
32.	Tasker Funeral Home	621 Central Avenue	–
33.	Dover City Hall	288 Central Avenue	1935

HISTORIC HOMES

<i>Building Name</i>	<i>Address</i>	<i>Date of Construction</i>
<u>Georgian (1700 – 1780)</u>		
1. John Gage House	30 Silver Street	1723
2. Tibbett's Tavern	45 Silver Street	1750
3. Mellen-Pierce House	53 Silver Street	1758
4. Schooner House	17 Portland Avenue	c. 1780
5. Crosby-Davis House	162 Central Avenue	1787
<u>Federal (1780 – 1820)</u>		
6. Hale-Lafayette House	5 Hale Street	1806
7. Sayward Bradley House	157 Central Avenue	1807
8. Dr. Jacob Kittredge House	242 Central Avenue	c. 1807
9. John Mann House	65 Silver Street	1812
10. Charles Woodman House	182 Central Avenue	1818
11. O'Neill House	2 Waldron Court	1827
12. Solomon Jenness House*	65 Main Street	1821
13. Kirkland Street Duplex	11-13 Kirkland Street	–
<i>*Includes elements from later stylistic periods.</i>		
<u>Greek Revival (1825 – 1860)</u>		
14. John Watson House*	153 & 155 Locust Street	1825
15. Gov. Noah Martin House**	16 Nelson Street	1827
16. Niles-Henderson-Owen House	163 Central Avenue	c. 1828-1838
17. Lincoln Building	113 Locust Street	1831
<i>*Early Classic Revival. Combines elements of Greek Revival and Italianate.</i>		
<i>**Also includes Gothic Revival elements</i>		
<u>Italianate (1840 – 1885)</u>		
18. Thomas Cushing Homestead	230 Washington Street	c. 1850
19. Charles Dorr House	35 Summer Street	c. 1880
20. 20 Grove Street	1880	
<u>Folk Victorian (1870 – 1910)</u>		
21. Dunn-Anderton House	14 & 16 Second Street	–

Second Empire (1855 – 1885)

22. Andrew Young House	37 Summer Street	c. 1870
23. Williams House	103 Silver Street	1871
24. Daniel Hall House	55 Summer Street	1877
25. Green Mansard	25 Portland Avenue	—
26. Louisa Chadwick House	269 Washington Street	c. 1878 - 1880
27. Hayes Hospital	45-47 Summer Street	1897
28. Summer Street Mansard	31 Summer Street	—

Stick Style (1860 – c. 1890)

29. Brown Stick Style	9-11 Hamilton Street	—
30. Charles H. Hobbes House	19-21 Lexington Street	1883
31. Langley-Porter-Hayes House	14-16 West Concord Street	c. 1886
32. White Stick Style	60 Summer Street	c. 1879
33. Broadway Stick Style	95 Broadway	—
34. Yellow Stick Style	12 Hamilton Street	—

Queen Anne (1880 – 1910)

35. Frank Wentworth House	202 Central Avenue	1870s
36. Valentine Mathes	11 Cushing Street	1889
37. Dr. Charles Tasker House	35 Cushing Street	c. 1898
38. Moses Paul House*	114 Locust Street	1834
39. AG Architects	634 Central Avenue	—
40. Queen Anne Townhouses	582-588 Central Avenue	—
41. White Queen Anne	14 Hamilton Street	—
42. Burnham House**	141-143 Silver Street	c. 1904

**Victorian embellishments added c. 1895*

***Combines Queen Anne and Shingle Style elements*

Colonial Revival (1880 – 1955)

43. Baptist Parsonage	14 Richmond Street	1905
44. Belknap Colonial	72 Belknap Street	—
45.	60 Cushing Street	—
46. Nelson Street Town Houses	2–8 Nelson Street	—

Craftsman (1905 – 1930) & Bungalow (1890 – 1930)

47. Blue Craftsman	574 Central Avenue	–
48. Green Foursquare	99 Belknap Street	–
49. Belknap Bungalow	92 Belknap Street	–
50. Kirkland Bungalow	21 Kirkland Street	c. 1916
51. Brown Craftsman	307 Washington Street	–

Sources:

Hindle, Thom. *The Old Photographs Series: Dover*. Augusta, Maine: Alan Sutton, 1994.

McAlester, Virginia & Lee. *A Field Guide to American Houses*. New York: Alfred A. Knopf, 1990.

Morgan, William. *The Abrams Guide to American House Styles*. New York: Abrams, 2004.

Rifkind, Carole. *A Field Guide to American Architecture*. New York: Plume (A division of Penguin Books), 1980.

Whiffen, Marcus. *American Architecture Since 1780: A Guide to the Styles, Revised Edition*. Cambridge, Massachusetts: The MIT Press, 1992.

Dover Heritage Walking Tours. Dover, NH: Dover Heritage Group, 1979-2003.

Civic and Commercial Architecture



1. Friends Meeting House



2. Hosea Sawyer Block



3. Belknap School



4. Orchard Street Firehouse



5. Brick House



6. Morrill Block

Civic and Commercial Architecture



7. St. John's Church



8. Wyskiel, Boc & Tillinghast



9. Bracewell Block 1879



10. Newberry's/National Block



11. National Block Detail



12. Merchants Row I

Civic and Commercial Architecture



13. Merchants Row II



14. Kelley's Row



15. Cocheco Mill Fanlight



16. Cocheco Mill Tower



17. Cocheco Mill



18. Cocheco Mill and Bridge

Civic and Commercial Architecture



19. Walker Block



20. St. Thomas Episcopal Church



21. Strafford County Courthouse 1889



22. Strafford Bank 1895



23. King Block



24. Woodman Block

Civic and Commercial Architecture



25. Woodman Block Storefront



26. Kennedy Building



27. McConnell Center



28. Dover Public Library



29. U.S. Post Office



30. Franklin Block/Montgomery Ward

Civic and Commercial Architecture



31. Café Ciabatta



32. Tasker Funeral Home

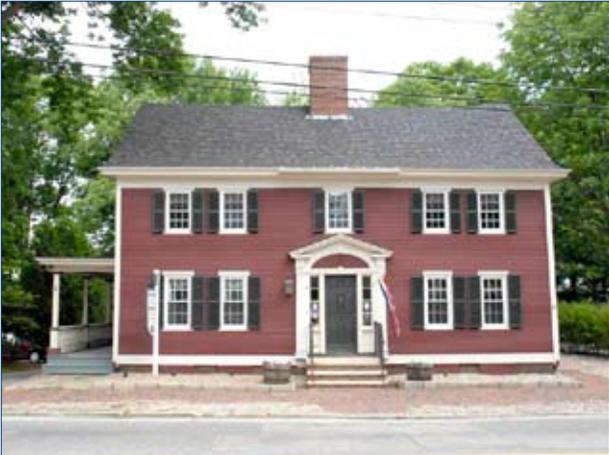


33. Dover City Hall

Historic Homes: Georgian (1700-1780)



1. John Gage House



2. Tibbett's Tavern



3. Mellen-Pierce House



4. Schooner House



5. Crosby-Davis House

Historic Homes: Federal (1780-1820)



6. Hale-Lafayette House



7. Sayward Bradley House



8. Dr. Jacob Kittredge House



9. John Mann House



10. Charles Woodman House



11. O'Neill House

Historic Homes: Federal (1780-1820)



12. Solomon Jenness House



13. Kirkland Street Duplex

Historic Homes: Greek Revival (1825-1860)



14. John Watson House



15. Gov. Noah Martin House



16. Niles-Henderson-Owen House



17. Lincoln Building

Historic Homes: Italianate (1840-1885)



18. Thomas Cushing Homestead



19. Charles Dorr House



20. 20 Grove Street

Historic Homes: Folk Victorian (1870-1910)



21. Dunn-Anderton House

Historic Homes: Second Empire (1855-1885)



22. Andrew Young House



23. Williams House



24. Daniel Hall House



25. Green Mansard



26. Louisa Chadwick House



27. Hayes Hospital

Historic Homes: Second Empire (1855-1885)



28. Summer Street Mansard

Historic Homes: Stick Style (1860-1890)



29. Brown Stick Style



30. Charles H. Hobbes House



31. Langley-Porter-Hayes House



32. White Stick Style



33. Broadway Stick Style



34. Yellow Stick Style

Historic Homes: Queen Anne (1880-1910)



35. Frank Wentworth House



36. Valentine Mathes



37. Dr. Charles Tasker House



38. Moses Paul House

Historic Homes: Queen Anne (1880-1910)



39. AG Architects



40. Queen Anne Townhouses



41. White Queen Anne



42. Burnham House

Historic Homes: Colonial Revival (1880-1955)



43. Baptist Parsonage



44. Belknap Colonial



45. 60 Cushing Street



46. Nelson Street Town Houses

Historic Homes: Craftsman (1905-1930) & Bungalow (1890-1930)



47. Blue Craftsman



48. Green Foursquare



49. Belknap Bungalow



50. Kirkland Bungalow



51. Brown Craftsman